

Section V.d. 2014 King County Countywide Competition Application for PSRC's FHWA Funds (STP/CMAQ)

❖ Nonmotorized Program

This application is available on the King County Department of Transportation website at:
<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

****Please read this section before completing the application****

The importance of complete and accurate information on every application cannot be overemphasized. The evaluation and scoring of all submitted projects will be based on the answers provided in this application. A project's suitability for countywide funding may be compromised if the application is found to have omissions or inaccuracies.

Sponsors of projects recommended for funding as a result of the competition should be aware that information provided on this application will be used in the future to monitor compliance with PSRC's adopted project tracking policies. It is also important to remember that funds are awarded to projects, not agencies. Please refer to PSRC's website for more information on the project tracking program: www.psrc.org/transportation/tip/tracking.

Submitting Applications

There is no set page limit for applications submitted to the countywide competition. It is important to provide complete, detailed responses, but please be as concise as possible. Additional supporting information such as maps and other diagrams are encouraged, but other attachments such as comprehensive plan materials are unnecessary. Attach your completed application to an email and send it to kcgrantcompetition@kingcounty.gov. All applications must be submitted by **11:59 p.m. Wednesday, May 7, 2014**.

Please note: the project budget spreadsheet is a required attachment, the budget spreadsheet can be downloaded from the following website:

<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

Definition of a project:

For the purposes of this competition, a project must be clearly defined by geographic limits and/or functionality. If a project contains multiple components, the sponsor must clearly indicate how they are logically connected to one another. A project with multiple geographic locations must demonstrate their functional relationship (for example, signal coordination work in various locations tied together through a traffic control center).

Projects that include multiple components or sponsors are allowed to be submitted, but the scope of work, funding amounts and schedules for each individual agency and/or component must be clearly identified at the time of application. If awarded PSRC funds, these projects may be separated into their individual components or lead agency in the regional Transportation Improvement Program. Each individual TIP project will be subject to PSRC's project tracking policies and will be administered according to the scope of work and funding awarded for each. **Note: a project may request only one funding source – either STP or CMAQ, but not both.**

If you have questions please contact Peter Heffernan at 206-477-3814 or peter.heffernan@kingcounty.gov

Section VI.c. 2014 King County Countywide STP/CMAQ Non-Motorized Application

This application is available on the King County Web site at <http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

PROJECT DESCRIPTION INFORMATION

1. Project Title: 116th Ave SE Sidewalk - from SE Petrovitsky Rd to SE 172nd Ln (extended)
(For roadway project titles: list facility name, limits and any other identifying words; e.g., SR-520HOV (104th Ave NE to 124th Ave NE)

2. Sponsoring Agency: City of Renton
Also identify co-sponsor(s):

3. Project Contact Person: Jim Seitz

Address: 1055 S Grady Way, Renton, WA 98057

Phone: 425-430-7245

Fax:

E-Mail: jseitz@rentonwa.gov

4. Project description. Please distinguish between the scope of the project and the justification and/or need for the project.

a. Project scope: Please describe clearly and concisely the individual components of this project. What will be the specific outcome of this project? What will be built, purchased or provided with this grant request? For example, if this is part of a larger project, please be specific as to what portion on which the grant funds will be used.

The grant will fund the design and construction of a missing link 5-foot wide sidewalk on the east side of 116th Ave SE. The project includes a vegetated buffer separating the sidewalk from the travelled roadway and pedestrian ramps per current ADA standards.

This project will establish a connection between the existing sidewalk at the intersection with SE Petrovitsky Rd (on the south) and the existing sidewalk fronting the post office (1,200 feet to the north at SE 172nd Ln extended).

b. Project justification, need or purpose: Please explain the intent, need or purpose of this project. What is the goal or desired outcome?

Justification:

116th Ave SE is a primary north-south arterial in the Benson Hill area, connecting neighbors to Cascade Village, Cascade and Benson Hill Elementary Schools, the post office, recreational activities and nearby unincorporated King County. Neighborhoods are physically disconnected from each other, as well as from retail and restaurant services. People cannot easily walk between residential areas and the commercial destinations and public transit.

The existing baseball field (adjacent to the Cascade Village shopping area) draw people for recreational events, the neighboring post office draws many users. Adding sidewalks on the east side of 116th Ave SE will contribute to the completion of an active transportation network, that serves the Benson Hill community, provides pedestrian access to transit, and is accessible to people of all ages, abilities, races, and income groups.

Goal:

Currently there is no sidewalk on the east side of 116th Ave SE from SE Petrovitsky Rd to the Post office. Pedestrians must walk in the shoulder area, adjacent to heavy vehicular traffic to from residential areas on the south to the shopping area and post office at the north end of the project.

The project completes a missing pedestrian link in the sidewalk network , connecting the Benson Hill neighborhoods north and south of SE Petrovitsky Rd and the Cascade Village shopping area including restaurants, the post office, the ball field, a Child Care Center at the north end of the project.

Purpose:

The project will enhance the safety and environment for pedestrian travelling along the east side of 116th Ave SE. 116th Ave SE is also heavily used by commuters south of Renton (Kent, Covington, Maple Valley, Auburn and Black Diamond) and an alternative route to bypass traffic congestion on parallel main arteries. Estimates from observation show that at least 75% of PM peak hour traffic is through commuter traffic. This directly affects the neighborhood as it creates a sense of an unsafe pedestrian environment.

Once the project is completed, pedestrians will be able to travel this link on a sidewalk separated from the travel lane by a vegetated buffer. The project will improve safety and comfort for pedestrian travel and provide environmental benefits, by investing in a "healthier" transportation system.

5. Project Location: 116th Ave SE

Answer the following questions if applicable:

a. Crossroad/landmark nearest to beginning of project: SE Petrovitsky Rd

(Identify landmark if no crossroad)

b. Crossroad/landmark nearest to end of project: SE 172nd Ln Extended

(Identify landmark if no crossroad)

6. Map: Include an 8 1/2" x 11" legible vicinity map (if applicable) with the completed application form. *(If unable to send map electronically, provide separately by fax or mail)*

7.	<p>Federal Functional Classification Code <i>(Select only one)</i></p> <p><i>Assistance in determining the functional classification of a project is available by calling Stephanie Rossi at 206-971-3054.</i></p>	
	<p><u>Rural Functional Classifications</u> <u>("under 5,000 population")</u> (Outside the federal-aid urbanized and federal-aid</p>	<p><u>Urban Functional Classifications</u> <u>("over 5,000 population")</u> (Inside the federal-aid urbanized and federal-aid</p>

urban areas)	urban areas)
<input type="checkbox"/> 00 Exception <input type="checkbox"/> 01 Principal Arterial - Interstate <input type="checkbox"/> 02 Principal Arterial <input type="checkbox"/> 06 Minor Arterial <input type="checkbox"/> 07 Major Collector <input type="checkbox"/> 08 Minor Collector <input type="checkbox"/> 09 Local Access <input type="checkbox"/> 21 Proposed Principal Arterial – Interstate <input type="checkbox"/> 22 Proposed Principal Arterial <input type="checkbox"/> 26 Proposed Minor Arterial <input type="checkbox"/> 27 Proposed Major Collector <input type="checkbox"/> 28 Proposed Minor Collector <input type="checkbox"/> 29 Proposed Local Access	<input type="checkbox"/> 00 Exception <input type="checkbox"/> 11 Principal Arterial – Interstate <input type="checkbox"/> 12 Principal Arterial – Expressway <input type="checkbox"/> 14 Principal Arterial <input checked="" type="checkbox"/> 16 Minor Arterial <input type="checkbox"/> 17 Collector <input type="checkbox"/> 19 Local Access <input type="checkbox"/> 31 Proposed Principal Arterial – Interstate <input type="checkbox"/> 32 Proposed Principal Arterial – Expressway <input type="checkbox"/> 34 Proposed Principal Arterial <input type="checkbox"/> 36 Proposed Minor Arterial <input type="checkbox"/> 37 Proposed Collector <input type="checkbox"/> 39 Proposed Local Access

NOTE: Federally Funded Projects. A roadway must be approved on the federally classified roadway system before projects on it may use federal transportation funds (this includes proposed new facilities). Projects which are on a roadway with a functional classification of 09, 19, 29 or 39 are not eligible to use federal transportation funds unless they are one of the exceptions listed below. If your project is an exception, identify its functional class code as “00”.

Examples of Exceptions:

- Any bicycle and/or pedestrian project.
- Projects not on a roadway and using CMAQ or other funds
- Any transit project, including equipment purchase and park-and-ride lot projects.

Important notice: The importance of complete and accurate information on every application cannot be overemphasized. The evaluation and scoring of all submitted projects will be based on the answers provided in this application. A project’s suitability for funding may be compromised if the application is found to have omissions or inaccuracies. In addition, sponsors of projects recommended for funding as a result of the competition should be aware that their application could be used in the future to evaluate the status of a project if it fails to comply with the requirements of the Puget Sound Regional Council’s (PSRC) Project Tracking program.

Projects receiving funding as a result of this competition: Funding distributed as a result of the 2014 STP/CMAQ King County Countywide Programs is awarded to projects, not to the sponsoring agency itself. Sponsors of projects that receive funds from this competition will be required to submit a more detailed TIPMOD or TIPNEW application, which will be due to the PSRC on July 7, 2014. Please note that these sponsors will also be asked to certify that they will comply with the conditions of the PSRC’s Project Tracking Program, as a condition of accepting funding. Failing to comply with this condition, and/or with the conditions established in the PSRC’s Project Tracking Program, may eventually result in the loss and/or transfer of funds to another countywide project.

Application length: Please be as brief as possible but provide sufficient information on your project, plus map(s) and/or other required supporting documents.

E-mail submissions are preferred: Attach your completed application to an e-mail and send to kcgrantcompetition@kingcounty.gov. Please name the file "(Agency): (Project title)" and in the e-mail subject line identify which Countywide program the application is being submitted (Small Jurisdiction, Large Jurisdiction, All Other, Preservation, Non-motorized). All applications will be posted to the King County Web site. All applications must be submitted by 11:59 p.m., Wednesday, May 7, 2014.

PROJECT EVALUATION INFORMATION

IMPORTANT INSTRUCTIONS: Projects will be evaluated and scored based on the information provided in Parts 1 and 2 that follow. Refer to Section IVb, Evaluation Criteria for Countywide Grant Programs, Non-Motorized Projects for information on how the projects will be evaluated.

- **Part 1:** Choose one of the two project categories that best fits your proposed project and complete Section A or B
- **Part 2:** Complete all Sections C through F

PROJECT EVALUATION: PART 1

Choose which of the two Centers categories your project falls under:

- Project is located within a Center
> *NOTE: Complete Section A, then proceed to Sections C through F in Part 2*
- Connecting Corridors
> *NOTE: Complete Section B, then proceed to Sections C through F in Part 2*

SECTION A: CENTERS

Complete this section if your project is a “Centers” project, then proceed to Part 2

A. Please explain how your project addresses the following:

• Center Development

- Describe how the project will advance or support non-motorized modes within the center.
- Describe how the project or program will enhance or support the potential for increased housing/employment densities in the center.
- Describe how the project furthers the objectives and aims of existing adopted policies and plans for the center.
- Describe the level of public access to the project (for example, current and future land use in the vicinity of the facility such as schools, residences, commercial, retail, tourist areas, etc. that would be expected to provide utilization of the facility).

• Project's Benefit to the Center

- Describe how the project remedies a current or anticipated problem for non-motorized modes within the center or reduces modal conflicts involving pedestrians and/or bicyclists?
- Describe the user groups who will benefit from this project (residents, commuters, employees, students, customers, tourists, seniors, people with disabilities, and those identified in the President’s Order for Environmental Justice¹) and how it provides users with non-motorized option for travel.
- Describe how the project or program will improve: (address each relevant area)
 - Bicycle facilities
 - Walkability
 - Public transit access
 - Landscape and/or streetscape

• Circulation and Safety within the Center

- Describe how the project improves access to major destinations or improves circulation within the center (home/work/school/other).
- Describe how the project or program completes a physical gap or completes an essential link in the non-motorized transportation network.
- Describe how the project improves safety or resolves an existing safety problem.
- Describe how the project extends or completes a regional or local bicycle or pedestrian system, and/or adds facilities to an existing bicycle and pedestrian system or network.

¹ The President’s Order for Environmental Justice states “each Federal agency shall make achieving environmental justice part of its mission by identifying and addressing, as appropriate, disproportionately high and adverse human health or environmental effects of its programs, policies and activities on minority populations and low-income populations.”

SECTION B: CONNECTING CORRIDORS

Complete this section if your project is a “Connecting Corridors” project, then proceed to Part 2.

A. Please explain how your project addresses the following:

• Benefit to Center

- Describe how the project provides users traveling to/from the center(s) with non-motorized options for travel.
- Describe how the project furthers the objectives and aims of adopted policies and plans for the center(s).
- Describe how this project will benefit or enhance support the development of the center(s). Does it support multiple centers?

The Benson Hill Community is a set of suburban neighborhoods loosely tied together because of surrounding barriers such as the Cedar River, Soos Creek Park, Interstate 405, Highway 167 and Highway 515. In the Benson Hill Community, Cascade Village and Benson Plaza are the only major commercial centers, and neither support community gathering or easy walking or bicycling.

116th Ave SE has been identified as the central spine in the Benson Hill Community, connecting neighbors to Cascade Village, Cascade and Benson Hill Elementary Schools, the post office and nearby unincorporated King County.

The project completes a missing pedestrian link in the sidewalk network , connecting the Benson Hill neighborhoods north and south of SE Petrovitsky Rd and the Cascade Village shopping area including restaurants, the post office, the ball field, a Child Care Center at the north end of the project.

The existing baseball field (adjacent to the Cascade Village shopping area) draw people for recreational events, the neighboring post office draws many users. Adding sidewalks on the east side of 116th Ave SE will contribute to the creation and completion of an active transportation network, that connects within the Benson Hill community, improving pedestrian access to transit, and is accessible to people of all ages, abilities, races, and income groups.

It will provide a missing link pedestrian connection and promote walking by enhancing the pedestrian environment, resulting in a safer and more attractive environment for non-motorized users. Currently pedestrians living south if the project site and destined to the Cascade Village shopping area and Post Office are faced with three options: a) walk in the unimproved shoulder on the east side of 116th Ave SE; b) use the existing sidewalk on the west side of 116th Ave SE and cross the heavily travelled road on an unmarked crosswalk in front of the shopping area (previous pedestrian injury accident reported at this location); and c) walk 1.5 mile out of their way (east on SE Petrovitsky Rd to 128th Ave SE, then north to SE 172nd St, then west to 116th Ave SE).

The project supports objectives and policies within Renton's Comprehensive Plan (pages XI-39-XI-40, attached) and the Benson Hill Community Plan (page 48, attached), as it enhances and improves the non-motorized circulation system within the City.

The project will support increased activity between the neighborhoods and Cascade Village, the post office, the ball field, schools within the Benson Hill Community. The project will encourage a walkable community by promoting pedestrian access to transit stops, schools, recreational activities, shopping and natural areas in Benson Hill.

- **System Continuity, Circulation and Safety**

- Describe how this project or program provides a "logical segment" that links to a center
- Describe how the project fills in a missing link or removes barriers to a center, and how the project extends or completes a regional or local bicycle or pedestrian system, and/or adds facilities to an existing bicycle and pedestrian system or network
- .Describe how the project improves safety and/or reduces modal conflict.

The project completes the gap in the sidewalk system accessing the Cascade Village shopping from the south. During the development of the Benson Hill Community Plan, residents, businesses and community stakeholders prioritize investments in services and infrastructure. Pedestrian investments along 116th Ave SE was ranked as a top priority as it would tie the community together. Participants in the development of the community plan wanted to see a better walking environment.

Completing the missing link sidewalk supports Renton and regional policies to encourage local and regional agencies to enhance non-motorized transportation by improving accessibility and safety and an attractive alternate mode of transportation to the single-occupancy vehicle.

One of the major benefits of this project is to improve safety for pedestrian travel with the installation of a missing link sidewalk separated from the travel lanes by a vegetated buffer and pedestrian ramps per current ADA standards.

Currently pedestrians living south if the project site and destined to the Cascade Village shopping area and Post Office are faced with three options: a) walk in the unimproved shoulder on the east side of 116th Ave SE; b) use the existing sidewalk on the west side of 116th Ave SE and cross the heavily travelled road on an unmarked crosswalk in front of the shopping area; and c) walk 1.5 mile out of their way (east on SE Petrovitsky Rd to 128th Ave SE, then north to SE 172nd St, then west to 116th Ave SE).

Within the last three years there were two (2) pedestrian injury accidents within the limits of the project. The installation of the sidewalk will provide a safer location for pedestrians to walk rather than walking along the existing unimproved shoulder area.

- **Sustainability**

- Describe how this project or program supports a long-term strategy to maximize the efficiency of the corridor and/or the bicycle or pedestrian network.
- Describe the user groups who will benefit from this project over time (residents, commuters, employees, students, customers, tourists, seniors, people with disabilities, and those identified in the President's Order for Environmental Justice) and how it provides users with non-motorized options for travel.

Completing the missing sidewalk link will provide a long-term solution to the community's pedestrian transportation needs.

This project furthers the adopted Benson Hill Community Plan's vision to grow into a well-connected, dynamic community that serves diverse population.

116th Ave SE is the central spine of the Benson Hill, connecting neighborhoods to Cascade Village, Cascade and Benson Hill Elementary Schools, trails and south King County.

Improvements on this central connection will tie Benson Hill together, specially to the south across SE Petrovitsky Rd. Along this spine, the heart of Benson Hill is the Cascade Village shopping area. The proposed improvements will provide a pedestrian connection from the neighborhood to the south to the "heart" (Cascade Village) of the Benson Hill Community.

The project will enhance pedestrian facilities for minority, low income and other protected classes. According to PSRC Comprehensive Opportunity map for Puget Sound, Benson Hill has lower "opportunity" than surrounding cities. This index is based on factors such as access to education, jobs, transportation alternatives, health food, social networks.

According to the 2009 census, 11% of the Benson Hill population live below the poverty level.

Residents within the Benson Hill area are racially/ethnically diverse, with a large (29.7%) portion speaking languages other than English, and of those, almost 7% linguistically isolated (no member of the household 14 years or older speaks english). Asian population is also present within the limits of the project, comprising 20% of the total population.

The project will greatly benefit seniors, by providing easy access to their community "on foot". According to the census, 8% of the population surrounding the project is 65 years of age or older.

The proposed sidewalk will assist the people of all ages, abilities, races and income groups with better access to shopping, the post office, schools, transit and recreation, by improving pedestrian safety, promoting a walkable community and reduction in air pollution.

PROJECT EVALUATION: PART 2

SECTION C: PROJECT READINESS/FINANCIAL PLAN

Once Section A or B in Part 1 has been completed, complete all of Part 2, Sections C through G.

2. Financial Plan

In this section, sponsors will address questions regarding the funding request, the total estimated project cost and schedule, and the project's readiness to obligate PSRC funds. Sponsors should be aware of the following information before completing this section:

Funding Request: Sponsors may request funding for any single project phase, but requests for multiple phases are limited to preliminary engineering plus the subsequent phase necessary. A sponsor may request funding for both preliminary engineering and right of way phases or preliminary engineering and construction phases, but not both right of way and construction phases.

Funding Requirements: A minimum of 13.5% of local matching funds is required for both Surface Transportation Program (STP) and Congestion Mitigation and Air Quality Improvement Program (CMAQ) funding. The combination of the requested PSRC funds plus all other funding must be adequate to fully fund that phase. Requests that do not result in a phase being fully funded will be considered ineligible for PSRC funding.

Obligation Requirements: Per PSRC's project tracking policies, all project phases awarded PSRC funds must obligate by June 1st of the program year selected. For more information, see PSRC's project tracking policies here. For more information on PSRC's project tracking program, please go to www.psrc.org/transportation/tip/tracking.

Required Match: A minimum of 13.5% match is required for both STP and CMAQ funds. Sponsors of projects awarded funds through this competition will be required to provide information on these matching funds at a later date.

2.1. Select only one funding source below, STP or CMAQ.

STP

CMAQ

2.2. Identify the amount requested by phase, and identify the estimated year of obligation (2015, 2016 or 2017).

<u>Phase</u>	<u>Amount</u>	<u>Estimated Year of Obligation</u>
PE/Design	\$102,000.00	2015
Construction	\$605,000.00	2016
[select phase]		

2.3 Identify the project phases that will be fully completed if requested funding is obtained:

Preliminary Engineering, Construction

2.4 Project Budget and Schedule

In this section you will be asked to provide information on the financial budget and schedule for the entire project. Please indicate amounts and sources of both secured and unsecured funds, by phase. Include all phases in the project, from start to finish, and indicate when each phase will be completed. The requested PSRC funds identified above must also be reflected in the Project Budget and Schedule spreadsheet. Use as many rows per phase as necessary to reflect the financial plan for each phase. The required table to provide this information is a separate Excel spreadsheet which you will need to download from following King County website:
<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

Attach the completed spreadsheet, along with this application, and submit via email to kcgrantcompetition@kingcounty.gov, by the deadline of 11:59 p.m. May 7, 2014. The Project Budget and Schedule spreadsheet form may be downloaded at:
<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

Project Readiness:

PSRC recognizes that the complexity of some projects can trigger a variety of prerequisites that must be satisfied before federal funding is typically eligible to obligate. These questions are designed to identify those requirements and assist sponsors to:

- Identify which obligation prerequisites and milestones apply to their specific project.
- Identify which of these have already been satisfied at time of application.
- Provide an explanation and realistic completion date for all obligation prerequisites and milestones not yet completed.

In the section below, sponsors will be asked to provide complete information on the status of necessary milestones for the project seeking PSRC funds. Past experience has shown that delays in one phase often result in a delay to subsequent phases. PSRC's project tracking policies require that funds be obligated within a set timeframe or be returned for redistribution. Consequently, sponsors are encouraged to carefully consider the complexity of their project and develop a project schedule that is realistic.

Based on the phase(s) for which PSRC funds are being requested, please answer the questions below. If funds are requested for Planning or Preliminary Engineering/Design only, this section is not required.

3. If funds are requested for Right of Way:

3.1 What is the status of Preliminary Engineering/Design?

- Is the PE/Design phase complete? No
- If not, identify all relevant milestones, including the current status and estimated completion date of each. For example:
 - What is the level of environmental documentation under the National Environmental Policy Act (NEPA) for this project?
 - Environmental Impact Statement (EIS)
 - Environmental Assessment (EA)

- Documented Categorical Exclusion (DCE)
- Categorical Exclusion (CE)
- Has the NEPA documentation been approved? Please provide the date of approval, or the anticipated date of completion. 09/2015
- At what stage of completion is your design?
 - Have Preliminary Plans been submitted to WSDOT for approval? No
 - If not, when is this milestone scheduled to be complete? 09/2015
 - When are Preliminary Plans expected to be approved? 11/2015
- Are there any other PE/Design milestones not listed above? Please identify and provide estimates dates of completion.

3.2 What is the status of Right of Way?

- How many parcels do you need? 0
- What is the zoning in the project area (e.g., commercial, residential, etc.)? Commercial corridor and multi-family
- Discuss the extent to which your schedule reflects the possibility of condemnation and the actions needed to pursue this. N/A
- Does your agency have experience in conducting right of way acquisitions of similar size and complexity? Yes
- If not, when do you expect a consultant to be selected, under contract, and ready to start? N/A
- Identify all relevant right of way milestones, including the current status and estimated completion date of each. For example:
 - True cost estimate of Right of Way N/A
 - Right of Way Plans (stamped) N/A
 - Relocation Plan (if applicable) N/A
 - Right of Way Certification N/A
 - Right of Way Acquisition N/A
 - Certification Audit by WSDOT Right of Way Analyst N/A
 - Relocation Certification, if applicable N/A

4. If funds are requested for Construction:

4.1 Complete sections 3.1 and 3.2 above.

4.2 What is the status of the milestones for the construction phase?

- Do you have an Engineer's Estimate? Please provide a copy if available. No
- Identify the environmental permits needed for the project and when they are scheduled to be acquired. NEPA and Hydraulic Project Approval (HPA) - 09/2015
- Is PS&E approved? Please provide the date of approval, or the date when PS&E is scheduled to be submitted for approval. 12/2015
- When is the project scheduled to go to ad? 02/2016

REMINDER: When you submit this application, please remember to also attach the Project Budget and Schedule spreadsheet and any maps or other project schematics, if applicable.

SECTION D: COMMUNITY SUPPORT/ENVIRONMENTAL JUSTICE/PLANNING

4. Please explain how your project addresses the following:

- Has the project been developed through a collaborative and inclusive planning process? Please describe. (If not, please describe how developed.)
- Is this project or program specifically identified in a local plan, transit plan, or regional plan. If not, is the project or program consistent with plan policies? Please provide citation of the corresponding policies and/or specific project references in the identified plan.
- Is this an important opportunity--if we fund this project or program now, what other benefits will result?
- Will an opportunity be missed if the project is not funded in this competition?

In 2011, the City of Renton and many Benson Hill residents, businesses, and community stakeholders partnered to create the Benson Hill Community Plan. Their collaborative effort strengthened connections between neighborhoods and the City, prioritized City investments in services and infrastructure, and supplied a vision for Benson Hill vetted by participants.

City staff met with residents in 2011 at several neighborhoods neighborhood picnics (Tiffany Park, Rolling Hills, Talbot Park/Victoria Park, and Cascade). People talked about what they liked and would like to see improved in Benson Hill.

After the City reaching out to individuals at the picnics, the Benson Hill Steering Committee (BHSC) was formed. The BHSC, comprised of residents, business owners, and community leaders, met three times during the initial phase of the project. In the first meeting, members were introduced to the planning process and discussed their priorities for Benson Hill. The second meeting focused on the best means of engaging more people in the process and helped to develop the outreach strategy for the plan. In the third meeting, members responded to the open house results and provided further input to staff on the draft vision statement for the plan.

In addition, a website was created in early 2012 (BensonPlan.org) to provide information about community planning in general and this project specifically, announce updates and events, and solicit input and feedback at various stages of the planning process.

In 2012 several public workshops were held to inform the community about the Benson Hill Community Plan. The community provided suggestions to the plan, made general comments and voted on the major concepts. As a result of this outreach, there was overwhelming support for improvements to 116th Ave SE. Participants wanted to see a better walking environment.

This project is identified as a planned element of the Benson Hill Community Plan, adopted on 2013. Project references are shown on page 48 of the Benson Hill Community Plan (attached). The project is also identified in the Comprehensive Plan, Transportation Element, page XI-21

If funded, this project will contribute to building a healthy and sustainable community through the installation of a missing link sidewalk on the east side of 116th Ave SE separated

from the travel lane by a vegetated buffer. Health impacts will be felt by pedestrians using this new transportation facility by encouraging walking in a more comfortable and safe environment. Encouraging the use of non-motorized modes of travel will improve health by increasing physical activity and reducing air pollution. This project provides opportunity to create a physically active community environment.

Not funding the project will result in a lost of opportunity to expeditiously complete a gap in the sidewalks for a major identified transportation corridor in the Benson Hill community.

SECTION E: MODE SHIFT

5. Please explain how your project addresses the following:

- Describe how the project eliminates SOV trips and induces a mode shift..
- Discuss the potential for non-motorized use. For example, use counts and/or user survey data from existing similar facilities combined with data on the population in the surrounding area to estimate the potential number of users of the proposed facility (more, less and why)
- Describe how the project connects to other non-motorized facilities.

The 116th Ave SE missing sidewalk link supports Renton and the regional policies to encourage non-motorized transportation by improving accessibility and safety and providing an attractive alternate mode of transportation to the single-occupancy vehicle.

The project completes a missing link in the sidewalk network, on the east side of 116th Ave SE that connects residential areas to Cascade Village commercial area, elementary schools, post office, recreational activities and provides direct access to transit. Completing this missing link would attract pedestrians who are reluctant to walk to these areas due to safety concerns, by having to walk in the unimproved shoulder, next to vehicular traffic.

One of the goals of the Benson Hill Community Plan is to build on the existing trail network to connect neighborhoods and provide recreational opportunities. This project will complete a missing link in a sidewalk network that will link to the future planned expansion of the trail network.

SECTION F: AIR QUALITY

NOTE: While project sponsors are not requested to provide detailed quantitative analyses at this time, those projects that are selected for CMAQ funds will be asked to assist staff in quantifying the benefits of their projects prior to TIP submittal.

6. Describe how your project will reduce emissions. Include discussion of the population served by the project – who will benefit, where and over what time period. Be as specific as possible and include examples. Answers will vary depending on the type of project, for example:

- Describe how your project will reduce VMT, either by eliminating or shortening vehicle trips;

- Describe how your project will result in a mode shift from SOVs to transit, carpool or nonmotorized;
- Describe how your project will result in an increase in transit ridership, either through new transit service or greater accessibility to transit;
- Describe how your project will improve the flow of traffic and reduce the amount of idling vehicles - how will this project relieve an existing problem;
- Describe how your project will reduce emissions through alternative fuels or vehicles.

The installation of a missing link sidewalk on the east side of 116th Ave SE reduces VMT by giving residents in the area a viable choice of completing part of or their entire trip by non-motorizing means, such as walking. The project will also improve access to transit as there is a bus stop within the project limits (Metro transit route 906).

By providing a safe and accessible environment for pedestrian travel, the project will offer users the opportunity to choose an alternative travel method other than vehicles. Some key reasons for mode shift from vehicles to walking include a significant perception of an increase in safety and convenience. The missing link sidewalk will encourage new users of non-motorized travel where sidewalk is currently non-existing or inconvenient.

Walking is an alternate choice to private automobiles. The cumulative effect of people traveling within the Benson Hill Community by walking can have an impact on the reduction of emissions in our region.



Renton - 116th Ave SE - SE Petrovitsky Rd to SE 172nd Ln (Extended)
Vicinity Map



Renton - 116th Ave SE - SE Petrovitsky Rd to SE 172nd Ln (Extended)
Project Location Map



BENSON HILL COMMUNITY PLAN

Adopted by Renton City Council October 14, 2013

Prepared by: MAKERS architecture and urban design

RENTON. AHEAD OF THE CURVE.

City of
Renton

Community & Economic Development





Comprehensive Plan

Adopted November 1, 2004
Ordinance 5099 & 5100



NON-MOTORIZED TRANSPORTATION

The non-motorized component of the City's Transportation Plan is designed to enhance the quality of urban life in Renton, to improve walking and bicycling safety, and to support the pedestrian and bicycle transportation modes as alternatives to the use of automobiles.

The plan recognizes that non-motorized facilities along roadways and trails may serve multiple functions, including commuting and recreation. The on-street elements are specified in the *City of Renton Comprehensive Citywide Walkway Program*, the *Renton Trails and Bicycle Master Plan* and as described later in this section. Off-street elements of the non-motorized transportation system are also specified in the *Renton Trails and Bicycle Master Plan* and in the *City of Renton Long Range Parks, Recreation Open Space and Trails Master Plan described in the Parks Element*.

1. Renton's existing transportation system is oriented towards accommodating cars, trucks, and buses rather than pedestrians or bicycles. The intent of the objectives and policies that follow is to provide guidelines for reevaluating the existing system and providing a better environment for walking and bicycling. Overall, pedestrian facilities throughout the City are intended to be upgraded.
2. More facilities are also needed for bicycle storage and parking in shopping areas, employment centers and in public places.
3. A better pedestrian network can be encouraged by creating an interconnected street system, developed to street standards, which include adequate walkways and street crossings. Traffic sanctuary islands and midblock crossings across busy arterials are also useful methods of improving the pedestrian environment.

Objectives

The Non-Motorized Chapter is based on the following objectives:

Objective T-K: Improve the non-motorized transportation system for both internal circulation and linkages to regional travel.

Objective T-L: Develop and maintain comprehensive trails system which provides non-motorized access throughout the City, maximizes public access to open space areas, and provides increased recreational opportunities for the public.

Objective T-M: Integrate Renton's non-motorized transportation needs into a comprehensive transportation system serving both local and regional users.

Objective T-N: Enhance and improve the non-motorized circulation system to, from, and within the City.

Objective T-O: Develop and designate appropriate pedestrian and bicycle commuter routes along existing minor arterial and collector arterial corridors.

Objective T-P: Improve the City's pedestrian and bicycle network to increase access to and circulation within the Urban Center - Downtown.

Policies

Policy T-47. Pedestrian and bicycle traffic should be accommodated within all areas of the City.

Policy T-48. Where right-of-way is available and bicycle demand justifies them, bicycle lanes should be marked and signed to accommodate larger volumes of bicycle traffic on select streets designated by the City.

Policy T-49. Pedestrian and bicycle movement across arterial intersections should be enhanced.

Policy T-50. Obstructions and conflicts that restrict pedestrian movement should be minimized on sidewalks, paths and other pedestrian areas.

Policy T-51. Convenient and safe pedestrian and bicycle access should be provided to and at the downtown Transit Center and all transit stops.

Policy T-52. Bicycle storage facilities and parking should be encouraged within development projects, in commercial areas and in parks.

Policy T-53. Secure bicycle parking facilities, such as bike lockers and bike racks should be provided at residential, commercial, and public establishments to encourage bicycle use.

Policy T-54. Streets and pedestrian paths in residential neighborhoods should be arranged as an interconnecting network and should connect to other streets.

Policy T-55. Pedestrian spaces should be emphasized and connected throughout the downtown.

Policy T-56. Pedestrians should be given priority use of sidewalks within the Urban Center – Downtown designated pedestrian areas.

Policy T-57. New pedestrian facilities should be compliant with the Americans with Disabilities Act, and existing facilities should be upgraded to improve accessibility.

Policy T-58. Non-motorized transportation should be developed in tandem with motorized transportation systems, recognizing issues such as safety, user diversity, and experiential diversity.

Policy T-59. Recognize the diversity of transportation modes and trip purposes of the following four groups: pedestrians, bicyclists, joggers and runners.

Policy T-60. Foot/bicycle separation should be provided wherever possible; however, where conflict occurs, foot traffic should be given preference.

Policy T-61. Adequate separation between non-motorized and motorized traffic should be provided to ensure safety.

Policy T-62. The adopted Renton Trails and Bicycle Master Plan and the adopted Long Range Parks, Recreation, Open Space, and Trails Plan should be coordinated with and be an integral component of the City's on-going transportation planning activities.

Policy T-63. Appropriate mitigation measures should be taken to address impacts on the City's transportation infrastructure. Contributions to the City's non-motorized circulation system will help alleviate such impacts.

Policy T-64. Bicycle and pedestrian facilities should be promoted not only as a viable means of transportation, but as an important method for maintaining overall health and fitness of Renton's citizens.

Existing Bicycle and Pedestrian Facilities

The City's existing non-motorized transportation system is comprised primarily of roadside sidewalks. Pedestrians have the exclusive use of sidewalks within business districts and have shared use with cyclists in other areas of the city.

Although the City Code requires that sidewalks be provided on all streets, many of the public streets were constructed before the existing code was enacted, and as a result, numerous roadways are currently without sidewalks. Streets needing sidewalks include both local and arterial roadways. The *City of Renton Comprehensive Citywide Walkway Study* addresses the sidewalks and walkways within the City. This report identifies a priority roster to construct "missing" sidewalk/walkway sections throughout the City. The priority evaluation system is based on four sidewalk users: 1) school children, 2) elderly persons, 3) transit riders, and 4) all other users.

Except within business districts, cyclists may use existing sidewalks, provided that they yield the right-of-way to pedestrians. Currently, Renton has a combined bicycle/pedestrian facility along Garden Avenue North (North 6th Street to North 8th Street) and North 8th Street (Garden Avenue North to Houser Way), and striped bicycle lanes on Southwest 16th Street (Oakesdale Avenue Southwest to Longacres Drive), on Oakesdale Avenue Southwest (SW 16th Street to SW 27th Street) on Duvall Avenue NE (NE 4th Street to NE 8th Street), and on NE 4th Street (east of Duvall Avenue NE).



116th Ave SE – Looking South
Existing unimproved shoulder.



116th Ave SE – Looking North
Pedestrian walking along unimproved shoulder.

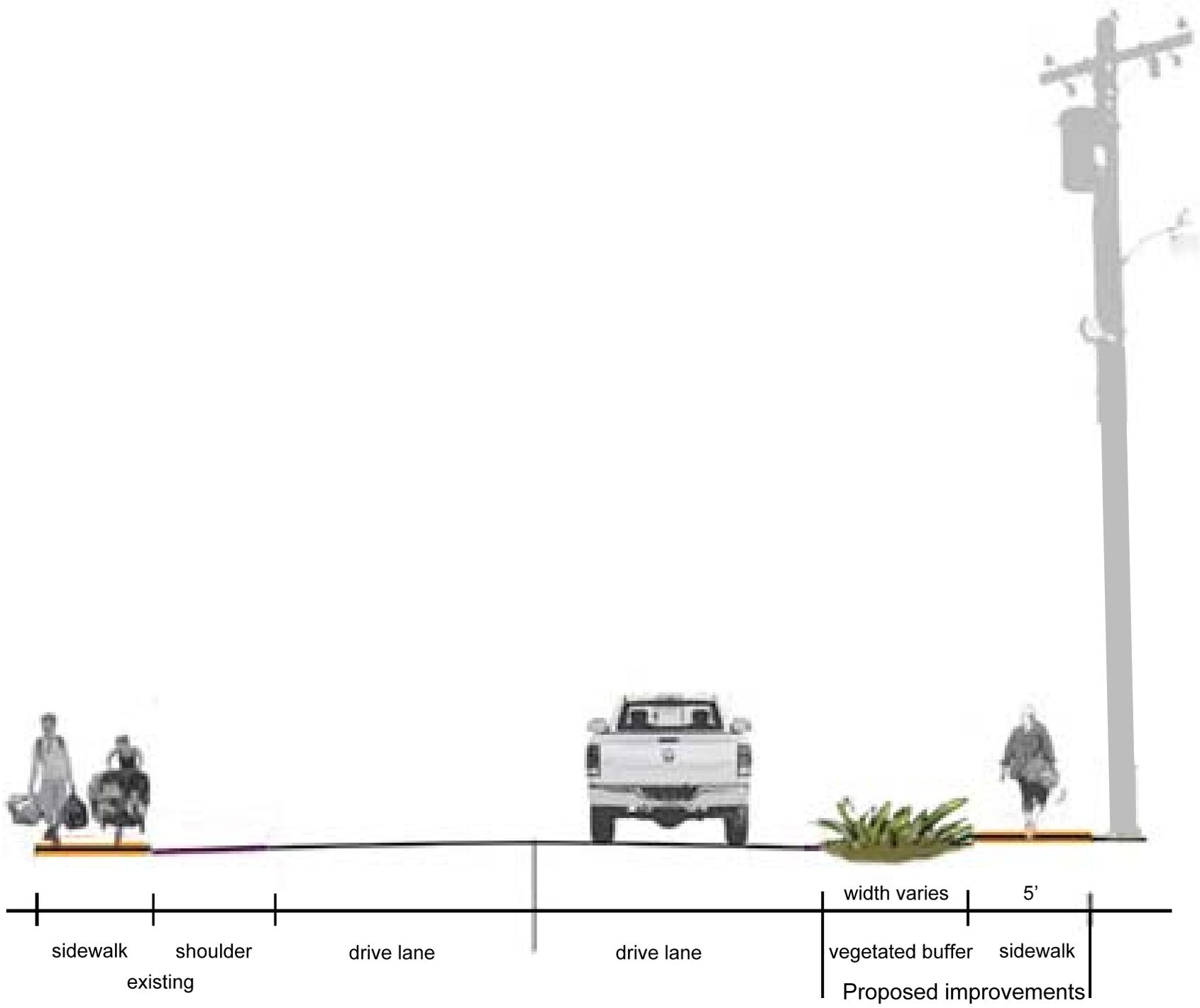


116th Ave SE – Looking South
Non-existing pedestrian facilities



116th Ave SE – Bus stop within project limits

RENTON – 116th Ave SE – SE Petrovitsky Rd to SE 172nd Ln (extended)
Existing Conditions



RENTON - 116th Ave SE - Typical Section

Section V.e. Total Estimated Project Cost and Schedule

Please fill in as many rows as needed in the tables below, to fully and accurately reflect your

Project Sponsor	City of Renton
Project Title	116th Ave SE Sidewalks - from SE Petrovitsky Rd to SE 172nd Ln (extended)

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Planning			
Planning			
Planning			

Planning TOTAL: \$ -

Estimated Planning Completion Date (month and year): _____

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
PE/Design	Local	Unsecured	\$ 16,000
PE/Design	Federal CMAQ	Unsecured	\$ 102,000
PE/Design			

Preliminary Engineering / Design TOTAL: \$ 118,000

Estimated PE/Design Completion Date (month and year): 11/1/2015

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Right of Way			
Right of Way			
Right of Way			

Right of Way TOTAL: \$ -

Estimated ROW Completion Date (month and year): _____

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Construction	Local	Unsecured	\$ 95,000
Construction	Federal CMAQ	Unsecured	\$ 605,000
Construction			

Construction TOTAL \$ 700,000

Estimated Construction Completion Date (month and year): 10/1/2016

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Other			
Other			

Other TOTAL: \$ -

Estimated Other Completion Date (month and year): _____

TOTAL Estimated Project Cost, All Phases: \$ 818,000

Estimated Project Completion Date (month and year): 10/1/2016

* Additional information on these categories may be found at
<http://www.psrc.org/assets/11214/FinancialConstraintGuidance.pdf>.