

Section V.d. 2014 King County Countywide Competition Application for PSRC's FHWA Funds (STP/CMAQ)

❖ Nonmotorized Program

This application is available on the King County Department of Transportation website at:
<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

****Please read this section before completing the application****

The importance of complete and accurate information on every application cannot be overemphasized. The evaluation and scoring of all submitted projects will be based on the answers provided in this application. A project's suitability for countywide funding may be compromised if the application is found to have omissions or inaccuracies.

Sponsors of projects recommended for funding as a result of the competition should be aware that information provided on this application will be used in the future to monitor compliance with PSRC's adopted project tracking policies. It is also important to remember that funds are awarded to projects, not agencies. Please refer to PSRC's website for more information on the project tracking program: www.psrc.org/transportation/tip/tracking.

Submitting Applications

There is no set page limit for applications submitted to the countywide competition. It is important to provide complete, detailed responses, but please be as concise as possible. Additional supporting information such as maps and other diagrams are encouraged, but other attachments such as comprehensive plan materials are unnecessary. Attach your completed application to an email and send it to kcgrantcompetition@kingcounty.gov. All applications must be submitted by **11:59 p.m. Wednesday, May 7, 2014**.

Please note: the project budget spreadsheet is a required attachment, the budget spreadsheet can be downloaded from the following website:

<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

Definition of a project:

For the purposes of this competition, a project must be clearly defined by geographic limits and/or functionality. If a project contains multiple components, the sponsor must clearly indicate how they are logically connected to one another. A project with multiple geographic locations must demonstrate their functional relationship (for example, signal coordination work in various locations tied together through a traffic control center).

Projects that include multiple components or sponsors are allowed to be submitted, but the scope of work, funding amounts and schedules for each individual agency and/or component must be clearly identified at the time of application. If awarded PSRC funds, these projects may be separated into their individual components or lead agency in the regional Transportation Improvement Program. Each individual TIP project will be subject to PSRC's project tracking policies and will be administered according to the scope of work and funding awarded for each. **Note: a project may request only one funding source – either STP or CMAQ, but not both.**

If you have questions please contact Peter Heffernan at 206-477-3814 or peter.heffernan@kingcounty.gov

Section VI.c. 2014 King County Countywide STP/CMAQ Non-Motorized Application

This application is available on the King County Web site at <http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

PROJECT DESCRIPTION INFORMATION

1. Project Title: Pacific Interurban Trail Extension - (3rd Ave SW to County Line Road)
(For roadway project titles: list facility name, limits and any other identifying words; e.g., SR-520HOV (104th Ave NE to 124th Ave NE)

2. Sponsoring Agency: City of Pacific
Also identify co-sponsor(s):

3. Project Contact Person: Jim Morgan

Address: 100 3rd Avenue SE, Pacific, WA 98047
Phone: (253)929-1115
Fax: (253)887-9910
E-Mail: jmorgan@ci.pacific.wa.us

4. Project description. Please distinguish between the scope of the project and the justification and/or need for the project.

a. Project scope: Please describe clearly and concisely the individual components of this project. What will be the specific outcome of this project? What will be built, purchased or provided with this grant request? For example, if this is part of a larger project, please be specific as to what portion on which the grant funds will be used.

The Project will upgrade the region's non-motorized transportation system by completing a significant portion of the Regional Interurban Trail (IUT) System within the City of Pacific from the current terminus at 3rd Avenue SW to County Line Road. Some elements of the trail have already been constructed by private developers.

The completed project will connect with the existing IUT that connects the Cities of Algona, Auburn, Kent, Tukwila and north to Seattle. Future segments will connect to the trail system to the City of Edgewood and towards Sumner and eventually beyond to Mount Rainier through the Cities of Puyallup, Orting, South Prairie, Wilkeson and Carbonado. It will also provide non-motorized connectivity to the developing Sumner/Pacific Manufacturing /Industrial Center, a short commute for the residents of the Cities of Auburn, Algona, Edgewood, Pacific, and Sumner, and the urbanized areas of Pierce and King Counties.

b. Project justification, need or purpose: Please explain the intent, need or purpose of this project. What is the goal or desired outcome?

The trail is proposed to be constructed within an easement on Puget Sound Energy (PSE) right-of-way adjacent to SR 167. The selected location reduces the number of pedestrian / bicycle facility and vehicular facility interfaces. This route will also provide an optional route to replace the bicycle facilities soon to be removed from SR 167. At this time a significant

portion of non-motorized transportation within the City of Pacific is relegated to the local surface streets and arterials with inadequate or missing links to other modes of transportation. Safety and the movement of freight, goods and people have become significant issues as the volumes of both motorized and non-motorized modes have increased because they occupy the same streets and arterials. This Project will provide separation of the transportation modes with safe crossings where they do intersect and establish links to other modes. These elements of the trail system will provide the stakeholders connections with King County Metro and Pierce Transit buses and Sound Transit’s Sounder trains, three of the region’s mass transit systems that currently serve the Cities of Auburn and Sumner.

5. Project Location: City of Pacific adjacent to SR167

Answer the following questions if applicable:

a. Crossroad/landmark nearest to beginning of project: 3rd Avenue SW

(Identify landmark if no crossroad)

b. Crossroad/landmark nearest to end of project: County Line Road

(Identify landmark if no crossroad)

6. Map: Include an 8 1/2” x 11” legible vicinity map (if applicable) with the completed application form. *(If unable to send map electronically, provide separately by fax or mail)*

7.	<p>Federal Functional Classification Code <i>(Select only one)</i></p> <p><i>Assistance in determining the functional classification of a project is available by calling Stephanie Rossi at 206-971-3054.</i></p>	
	<p style="text-align: center;"><u>Rural Functional Classifications</u> <u>(“under 5,000 population”)</u></p> <p style="text-align: center;">(Outside the federal-aid urbanized and federal-aid urban areas)</p> <p><input type="checkbox"/> 00 Exception</p> <p><input type="checkbox"/> 01 Principal Arterial - Interstate</p> <p><input type="checkbox"/> 02 Principal Arterial</p> <p><input type="checkbox"/> 06 Minor Arterial</p> <p><input type="checkbox"/> 07 Major Collector</p> <p><input type="checkbox"/> 08 Minor Collector</p> <p><input type="checkbox"/> 09 Local Access</p> <p><input type="checkbox"/> 21 Proposed Principal Arterial – Interstate</p> <p><input type="checkbox"/> 22 Proposed Principal Arterial</p> <p><input type="checkbox"/> 26 Proposed Minor Arterial</p> <p><input type="checkbox"/> 27 Proposed Major Collector</p> <p><input type="checkbox"/> 28 Proposed Minor Collector</p> <p><input type="checkbox"/> 29 Proposed Local Access</p>	<p style="text-align: center;"><u>Urban Functional Classifications</u> <u>(“over 5,000 population”)</u></p> <p style="text-align: center;">(Inside the federal-aid urbanized and federal-aid urban areas)</p> <p><input checked="" type="checkbox"/> 00 Exception</p> <p><input type="checkbox"/> 11 Principal Arterial – Interstate</p> <p><input type="checkbox"/> 12 Principal Arterial – Expressway</p> <p><input type="checkbox"/> 14 Principal Arterial</p> <p><input type="checkbox"/> 16 Minor Arterial</p> <p><input type="checkbox"/> 17 Collector</p> <p><input type="checkbox"/> 19 Local Access</p> <p><input type="checkbox"/> 31 Proposed Principal Arterial – Interstate</p> <p><input type="checkbox"/> 32 Proposed Principal Arterial – Expressway</p> <p><input type="checkbox"/> 34 Proposed Principal Arterial</p> <p><input type="checkbox"/> 36 Proposed Minor Arterial</p> <p><input type="checkbox"/> 37 Proposed Collector</p> <p><input type="checkbox"/> 39 Proposed Local Access</p>

NOTE: Federally Funded Projects. A roadway must be approved on the federally classified roadway system before projects on it may use federal transportation funds (this includes proposed new facilities). Projects which are on a roadway with a functional classification of 09, 19, 29 or 39 are not eligible to use federal transportation funds unless they are one of the exceptions listed below. If your project is an exception, identify its functional class code as "00".

Examples of Exceptions:

- Any bicycle and/or pedestrian project.
- Projects not on a roadway and using CMAQ or other funds
- Any transit project, including equipment purchase and park-and-ride lot projects.

Important notice: The importance of complete and accurate information on every application cannot be overemphasized. The evaluation and scoring of all submitted projects will be based on the answers provided in this application. A project's suitability for funding may be compromised if the application is found to have omissions or inaccuracies. In addition, sponsors of projects recommended for funding as a result of the competition should be aware that their application could be used in the future to evaluate the status of a project if it fails to comply with the requirements of the Puget Sound Regional Council's (PSRC) Project Tracking program.

Projects receiving funding as a result of this competition: Funding distributed as a result of the 2014 STP/CMAQ King County Countywide Programs is awarded to projects, not to the sponsoring agency itself. Sponsors of projects that receive funds from this competition will be required to submit a more detailed TIPMOD or TIPNEW application, which will be due to the PSRC on July 7, 2014. Please note that these sponsors will also be asked to certify that they will comply with the conditions of the PSRC's Project Tracking Program, as a condition of accepting funding. Failing to comply with this condition, and/or with the conditions established in the PSRC's Project Tracking Program, may eventually result in the loss and/or transfer of funds to another countywide project.

Application length: Please be as brief as possible but provide sufficient information on your project, plus map(s) and/or other required supporting documents.

E-mail submissions are preferred: Attach your completed application to an e-mail and send to kcgrantcompetition@kingcounty.gov. Please name the file "(Agency): (Project tile)" and in the e-mail subject line identify which Countywide program the application is being submitted (Small Jurisdiction, Large Jurisdiction, All Other, Preservation, Non-motorized). All applications will be posted to the King County Web site. All applications must be submitted by 11:59 p.m., Wednesday, May 7, 2014.

PROJECT EVALUATION INFORMATION

IMPORTANT INSTRUCTIONS: Projects will be evaluated and scored based on the information provided in Parts 1 and 2 that follow. Refer to Section IVb, Evaluation Criteria for Countywide Grant Programs, Non-Motorized Projects for information on how the projects will be evaluated.

- **Part 1:** Choose one of the two project categories that best fits your proposed project and complete Section A or B
- **Part 2:** Complete all Sections C through F

PROJECT EVALUATION: PART 1

Choose which of the two Centers categories your project falls under:

- Project is located within a Center
> *NOTE: Complete Section A, then proceed to Sections C through F in Part 2*
- Connecting Corridors
> *NOTE: Complete Section B, then proceed to Sections C through F in Part 2*

SECTION A: CENTERS

Complete this section if your project is a “Centers” project, then proceed to Part 2

A. Please explain how your project addresses the following:

• Center Development

- Describe how the project will advance or support non-motorized modes within the center.
- Describe how the project or program will enhance or support the potential for increased housing/employment densities in the center.
- Describe how the project furthers the objectives and aims of existing adopted policies and plans for the center.
- Describe the level of public access to the project (for example, current and future land use in the vicinity of the facility such as schools, residences, commercial, retail, tourist areas, etc. that would be expected to provide utilization of the facility).

• Project's Benefit to the Center

- Describe how the project remedies a current or anticipated problem for non-motorized modes within the center or reduces modal conflicts involving pedestrians and/or bicyclists?
- Describe the user groups who will benefit from this project (residents, commuters, employees, students, customers, tourists, seniors, people with disabilities, and those identified in the President’s Order for Environmental Justice¹) and how it provides users with non-motorized option for travel.
- Describe how the project or program will improve: (address each relevant area)
 - Bicycle facilities
 - Walkability
 - Public transit access
 - Landscape and/or streetscape

• Circulation and Safety within the Center

- Describe how the project improves access to major destinations or improves circulation within the center (home/work/school/other).
- Describe how the project or program completes a physical gap or completes an essential link in the non-motorized transportation network.
- Describe how the project improves safety or resolves an existing safety problem.
- Describe how the project extends or completes a regional or local bicycle or pedestrian system, and/or adds facilities to an existing bicycle and pedestrian system or network.

¹ The President’s Order for Environmental Justice states “each Federal agency shall make achieving environmental justice part of its mission by identifying and addressing, as appropriate, disproportionately high and adverse human health or environmental effects of its programs, policies and activities on minority populations and low-income populations.”

SECTION B: CONNECTING CORRIDORS

Complete this section if your project is a “Connecting Corridors” project, then proceed to Part 2.

A. Please explain how your project addresses the following:

• **Benefit to Center**

- Describe how the project provides users traveling to/from the center(s) with non-motorized options for travel.
- Describe how the project furthers the objectives and aims of adopted policies and plans for the center(s).
- Describe how this project will benefit or enhance support the development of the center(s). Does it support multiple centers?

The Interurban Trail has been a non-motorized commuting and recreation facility for more than three decades. It will also provide non-motorized connectivity to the developing Sumner/Pacific Manufacturing /Industrial Center, a short commute for the residents of the Cities of Auburn, Algona, Edgewood, Pacific, and Sumner, and the urbanized areas of Pierce and King Counties.

• **System Continuity, Circulation and Safety**

- Describe how this project or program provides a "logical segment" that links to a center
- Describe how the project fills in a missing link or removes barriers to a center, and how the project extends or completes a regional or local bicycle or pedestrian system, and/or adds facilities to an existing bicycle and pedestrian system or network
- .Describe how the project improves safety and/or reduces modal conflict.

The Interurban Trail (King County) and the Foothills Trail (Pierce County) are vital elements of the non-motorized transportation infrastructure in the Puget Sound area. They are currently not connected. Constructing this segment of the trail will be one more step towards connecting the King and Pierce County Trail Systems.

• **Sustainability**

- Describe how this project or program supports a long-term strategy to maximize the efficiency of the corridor and/or the bicycle or pedestrian network.
- Describe the user groups who will benefit from this project over time (residents, commuters, employees, students, customers, tourists, seniors, people with disabilities, and those identified in the President’s Order for Environmental Justice) and how it provides users with non-motorized options for travel.

Currently, bicycles are permitted on SR 167. However, the proposed new HOT lanes for SR 167 will require that bicycles will no longer be permitted on SR 167. The alternative routes are narrow roads will heavy truck traffic.

PROJECT EVALUATION: PART 2

SECTION C: PROJECT READINESS/FINANCIAL PLAN

Once Section A or B in Part 1 has been completed, complete all of Part 2, Sections C through G.

2. Financial Plan

In this section, sponsors will address questions regarding the funding request, the total estimated project cost and schedule, and the project's readiness to obligate PSRC funds. Sponsors should be aware of the following information before completing this section:

Funding Request: Sponsors may request funding for any single project phase, but requests for multiple phases are limited to preliminary engineering plus the subsequent phase necessary. A sponsor may request funding for both preliminary engineering and right of way phases or preliminary engineering and construction phases, but not both right of way and construction phases.

Funding Requirements: A minimum of 13.5% of local matching funds is required for both Surface Transportation Program (STP) and Congestion Mitigation and Air Quality Improvement Program (CMAQ) funding. The combination of the requested PSRC funds plus all other funding must be adequate to fully fund that phase. Requests that do not result in a phase being fully funded will be considered ineligible for PSRC funding.

Obligation Requirements: Per PSRC's project tracking policies, all project phases awarded PSRC funds must obligate by June 1st of the program year selected. For more information, see PSRC's project tracking policies here. For more information on PSRC's project tracking program, please go to www.psrc.org/transportation/tip/tracking.

Required Match: A minimum of 13.5% match is required for both STP and CMAQ funds. Sponsors of projects awarded funds through this competition will be required to provide information on these matching funds at a later date.

2.1. Select only one funding source below, STP or CMAQ.

STP

CMAQ

2.2. Identify the amount requested by phase, and identify the estimated year of obligation (2015, 2016 or 2017).

<u>Phase</u>	<u>Amount</u>	<u>Estimated Year of Obligation</u>
PE/Design	\$0.00	0
Construction	\$865,000.00	2015
[select phase]		

2.3 Identify the project phases that will be fully completed if requested funding is obtained:

PE and CN

2.4 Project Budget and Schedule

In this section you will be asked to provide information on the financial budget and schedule for the entire project. Please indicate amounts and sources of both secured and unsecured funds, by phase. Include all phases in the project, from start to finish, and indicate when each phase will be completed. The requested PSRC funds identified above must also be reflected in the Project Budget and Schedule spreadsheet. Use as many rows per phase as necessary to reflect the financial plan for each phase. The required table to provide this information is a separate Excel spreadsheet which you will need to download from following King County website:
<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

Attach the completed spreadsheet, along with this application, and submit via email to kcgrantcompetition@kingcounty.gov, by the deadline of 11:59 p.m. May 7, 2014. The Project Budget and Schedule spreadsheet form may be downloaded at:
<http://www.kingcounty.gov/transportation/kcdot/PlanningAndPolicy/RegionalTransportationPlanning/2014KCountywideCFP.aspx>

Project Readiness:

PSRC recognizes that the complexity of some projects can trigger a variety of prerequisites that must be satisfied before federal funding is typically eligible to obligate. These questions are designed to identify those requirements and assist sponsors to:

- Identify which obligation prerequisites and milestones apply to their specific project.
- Identify which of these have already been satisfied at time of application.
- Provide an explanation and realistic completion date for all obligation prerequisites and milestones not yet completed.

In the section below, sponsors will be asked to provide complete information on the status of necessary milestones for the project seeking PSRC funds. Past experience has shown that delays in one phase often result in a delay to subsequent phases. PSRC's project tracking policies require that funds be obligated within a set timeframe or be returned for redistribution. Consequently, sponsors are encouraged to carefully consider the complexity of their project and develop a project schedule that is realistic.

Based on the phase(s) for which PSRC funds are being requested, please answer the questions below. If funds are requested for Planning or Preliminary Engineering/Design only, this section is not required.

3. If funds are requested for Right of Way:

3.1 What is the status of Preliminary Engineering/Design?

- Is the PE/Design phase complete? No
- If not, identify all relevant milestones, including the current status and estimated completion date of each. For example:
 - What is the level of environmental documentation under the National Environmental Policy Act (NEPA) for this project?
 - Environmental Impact Statement (EIS)
 - Environmental Assessment (EA)

- Documented Categorical Exclusion (DCE)
 - Categorical Exclusion (CE)
- Has the NEPA documentation been approved? Please provide the date of approval, or the anticipated date of completion. Expected by 03/2015
- At what stage of completion is your design?
 - Have Preliminary Plans been submitted to WSDOT for approval? Not yet.
 - If not, when is this milestone scheduled to be complete? 09/2014
 - When are Preliminary Plans expected to be approved? 12/2014
- Are there any other PE/Design milestones not listed above? Please identify and provide estimates dates of completion. N/A

3.2 What is the status of Right of Way?

- How many parcels do you need? 0
- What is the zoning in the project area (e.g., commercial, residential, etc.)? Residential
- Discuss the extent to which your schedule reflects the possibility of condemnation and the actions needed to pursue this. No condemnation required.
- Does your agency have experience in conducting right of way acquisitions of similar size and complexity? N/A
- If not, when do you expect a consultant to be selected, under contract, and ready to start? N/A
- Identify all relevant right of way milestones, including the current status and estimated completion date of each. For example:
 - True cost estimate of Right of Way N/A
 - Right of Way Plans (stamped) No
 - Relocation Plan (if applicable) N/A
 - Right of Way Certification Expected by 03/2015
 - Right of Way Acquisition Not required
 - Certification Audit by WSDOT Right of Way Analyst Expected by 06/2015
 - Relocation Certification, if applicable N/A

4. If funds are requested for Construction:

4.1 Complete sections 3.1 and 3.2 above.

4.2 What is the status of the milestones for the construction phase?

- Do you have an Engineer's Estimate? Please provide a copy if available.
Attached
- Identify the environmental permits needed for the project and when they are scheduled to be acquired. NEPA - 06/2015
- Is PS&E approved? Please provide the date of approval, or the date when PS&E is scheduled to be submitted for approval. Design 50%. Approval by 12/2014
- When is the project scheduled to go to ad? 12/2015

REMINDER: When you submit this application, please remember to also attach the Project Budget and Schedule spreadsheet and any maps or other project schematics, if applicable.

SECTION D: COMMUNITY SUPPORT/ENVIRONMENTAL JUSTICE/PLANNING

4. Please explain how your project addresses the following:

- Has the project been developed through a collaborative and inclusive planning process? Please describe. (If not, please describe how developed.)
- Is this project or program specifically identified in a local plan, transit plan, or regional plan. If not, is the project or program consistent with plan policies? Please provide citation of the corresponding policies and/or specific project references in the identified plan.
- Is this an important opportunity--if we fund this project or program now, what other benefits will result?
- Will an opportunity be missed if the project is not funded in this competition?

This trail project has been an element of the City six year transportation plan for several years. The proposed trail segment is a logical next step in the process to connect the King and Pierce County trail systems. This section was identified in the 1996 Sumner/Pacific Trail Master Plan, adopted by Pacific through Res. #428 on September 9, 1996, and has been shown in our Comp Plan since. The Parks, Open Space, Recreation and Trails Chapter of the Comp Plan was last updated and certified by RCO in 2011. The Interurban Trail is shown on the map also adopted by Ordinance No. 1794 (January 10, 2011). The City of Pacific is currently requesting bids to construct a trail on Valentine Avenue and will request bids late 2014 or early 2015 for trail construction on Stewart Road. Both of these trails will connect with this extension and the Sumner trail network.

SECTION E: MODE SHIFT

5. Please explain how your project addresses the following:

- Describe how the project eliminates SOV trips and induces a mode shift..
- Discuss the potential for non-motorized use. For example, use counts and/or user survey data from existing similar facilities combined with data on the population in the surrounding area to estimate the potential number of users of the proposed facility (more, less and why)
- Describe how the project connects to other non-motorized facilities.

This project will connect the existing Interurban Trail to recently constructed and soon to be constructed segments of trail in Pierce County to extend to the Foothills Trail. Providing these trail connections will permit safe access to the Sounder Train Stations in Auburn and Sumner.

SECTION F: AIR QUALITY

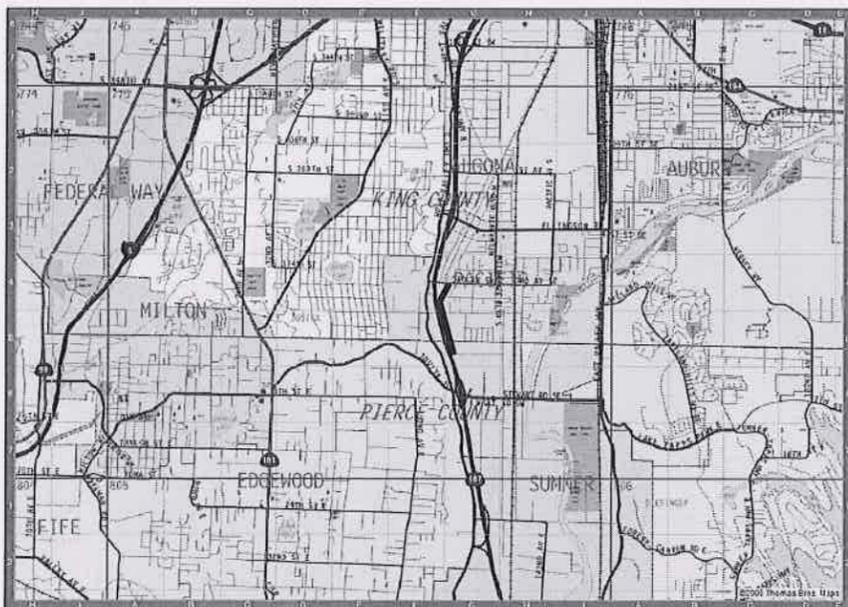
NOTE: While project sponsors are not requested to provide detailed quantitative analyses at this time, those projects that are selected for CMAQ funds will be asked to assist staff in quantifying the benefits of their projects prior to TIP submittal.

6. Describe how your project will reduce emissions. Include discussion of the population served by the project – who will benefit, where and over what time period. Be as specific as possible and include examples. Answers will vary depending on the type of project, for example:

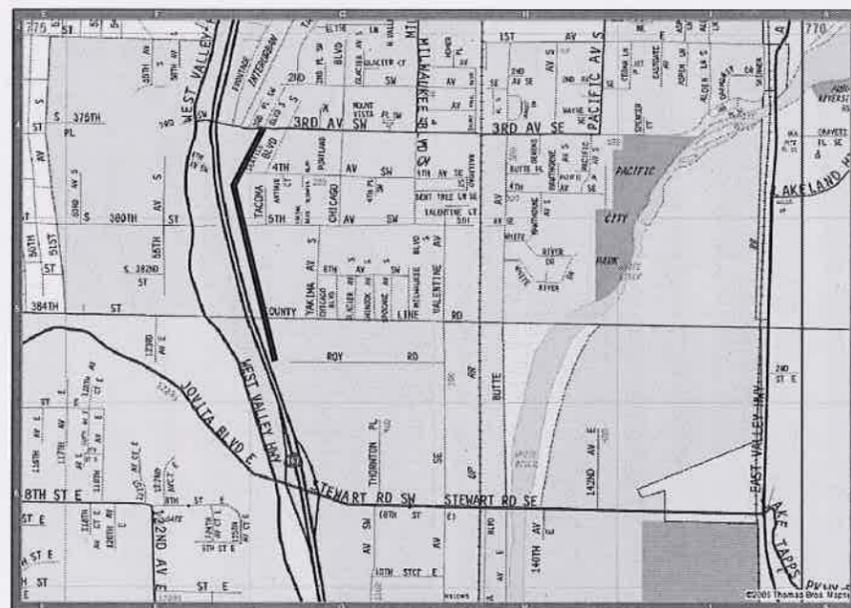
- Describe how your project will reduce VMT, either by eliminating or shortening vehicle trips;
- Describe how your project will result in a mode shift from SOVs to transit, carpool or nonmotorized;
- Describe how your project will result in an increase in transit ridership, either through new transit service or greater accessibility to transit;
- Describe how your project will improve the flow of traffic and reduce the amount of idling vehicles - how will this project relieve an existing problem;
- Describe how your project will reduce emissions through alternative fuels or vehicles.

Currently there is not a continuous route of non-motorized facilities connecting the Interurban Trail of King County with the Foothill Trail of Pierce County. Cyclists are required to share the narrow traffic corridors with large freight vehicles. Sharing of these facilities reducing speeds increasing emissions. The requirement to share facilities also reduces the number of non-motorized users, because of safety

INTERURBAN TRAIL EXTENSION PSE CORRIDOR PACIFIC, WASHINGTON



LOCATION MAP 



VICINITY MAP 

INDEX TO DRAWINGS		
SHT NO.	DWG NO.	SHEET TITLE
GENERAL		
1	G1	COVER SHEET
2	G2	GENERAL NOTES, LEGEND AND ABBREVIATIONS
DEMOLITION/TESC		
3	DM1	ELECTRIC AVE DEMOLITION AND TESC PLAN
ALIGNMENT		
4	AL1	ELECTRIC AVE TRAIL GRADING, ALIGNMENT AND SIGNAGE PLAN STA 100+00 TO STA 108+00
5	AL2	PSE AVE TRAIL GRADING, ALIGNMENT AND SIGNAGE PLAN STA 108+00 TO STA 116+00
6	AL3	PSE AVE TRAIL GRADING, ALIGNMENT AND SIGNAGE PLAN STA 116+00 TO STA 124+00
7	AL4	PSE AVE TRAIL GRADING, ALIGNMENT AND SIGNAGE PLAN STA 124+00 TO STA 132+00
8	AL5	PSE AVE TRAIL GRADING, ALIGNMENT AND SIGNAGE PLAN STA 132+00 TO ROY STREET
BRIDGE		
9	BR1	BRIDGE AT MILWAUKEE DITCH
WETLANDS		
10	WT1	FINAL WETLAND MITIGATION PLAN
11	WT2	FINAL WETLAND MITIGATION PLAN
STREAM		
12	ST1	FINAL STREAM BUFFER RESTORATION PLANTING PLAN
13	ST2	FINAL STREAM BUFFER RESTORATION PLANTING PLAN
LANDSCAPE		
14	LS1	PLANTING NOTES AND DETAILS
DETAILS		
15	DT1	TYPICAL SECTIONS
16	DT2	TYPICAL DETAILS
17	DT3	TYPICAL DETAILS
STORMWATER		
18	SD1	STORMWATER CONVEYANCE PLANS

50% REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

LAYOUT: G1 PATH: U:\Projects\Projects\214-3805-City of Pacific\214-3805-04-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawings\ PLOTTED BY: stumpor DATE: Friday, November 11, 2011 3:52:15 PM

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

ONE INCH AT FULL SCALE.
IF NOT, SCALE ACCORDINGLY
FILE NAME: PU3805004P08-G
JOB No: 214-3805-004
DATE: NOVEMBER 2011

PRELIMINARY

Parametrix
ENGINEERING, PLANNING, ENVIRONMENTAL SCIENCES
1019 39th AVENUE S.E., SUITE 100
PUYALLUP, WASHINGTON 98374
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PROJECT NAME
INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD
PACIFIC, WASHINGTON

COVER SHEET

SHEET NO.
1 OF XX
G1

LAYOUT: GZ
 PATH: U:\Projects\Projects\Clients\3805-City of Pacific\214-3805-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawings\
 PLOTTED BY: stumpf DATE: Friday, November 11, 2011 3:52:38 PM

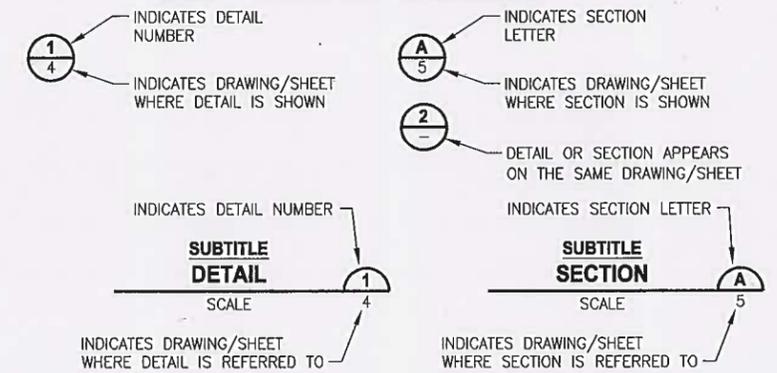
LEGEND

DESCRIPTION	PROPOSED	EXISTING
FENCE		
WATER LINE		
STORM DRAIN		
SANITARY SEWER		
GAS		
POWER		
TELEPHONE		
RIGHT-OF-WAY		
CONSTRUCTION EASEMENT		
EDGE OF PAVEMENT		
EDGE OF GRAVEL		
MAIL BOX		
WATER SERVICE BOX		
FIRE HYDRANT		
SHRUBBERY		
TREE-DECIDUOUS		
TREE-CONIFER		
SS MANHOLE		
CATCH BASIN TYPE II		
CATCH BASIN TYPE 1 OR TYPE 50		
ROCKERY		
ROOF DRAIN		
GAS VAULT		
GAS METER		
GAS VALVE		
TEL J BOX		
TEL VAULT		
POWER VAULT		
UTILITY POLE		
GUY WIRE		
SIGN		
LIGHT STD		
STREET LIGHT		

ABBREVIATIONS

APPROX.	APPROXIMATE	FCA	FLANGE COUPLING ADAPTER
ASPH	ASPHALT	FG	FINISH GRADE
ASSY	ASSEMBLY	FH	FIRE HYDRANT
AVE	AVENUE	FIG	FIGURE
AVG	AVERAGE	FIN	FINISH, FINISHED
BC	BEGINNING OF CURVE, BOLT CIRCLE	FL	FLOW LINE
BF	BLIND FLANGE	FLG	FLANGE, FLANGED
BLDG	BUILDING	FM	FORCE MAIN
BLVD	BOULEVARD	FT	FEET, FOOT
BM	BEAM, BENCH MARK	FUT	FUTURE
BO	BLOW OFF	G	GAS
BOC	BACK OF CURB	GALV	GALVANIZED
BOW	BACK OF WALK	GR	GRADE
BRG	BEARING	GV	GATE VALVE
BUV	BUTTERFLY VALVE	HDPE	HIGH DENSITY POLYETHYLENE
CAP	CAPACITY	HORIZ	HORIZONTAL
CB	CATCH BASIN	HT	HEIGHT
CCP	CONCRETE CYLINDER PIPE	IE	INVERT ELEVATION
CDF	CONTROLLED DENSITY FILL	IN	INCH
CEM	CEMENT	INCL	INCLUDE, INCLUDING
CHV	CHECK VALVE	INFO	INFORMATION
CI	CAST IRON	INSTL	INSTALL, INSTALLATION
CIP	CAST IN PLACE, CAST IRON PIPE	INT	INTERIOR, INTERSECTION
CIR	CIRCLE	INV	INVERT
CJ	CONSTRUCTION JOINT	LF	LINEAR FEET, LINEAR FOOT
CL	CENTERLINE	LT	LEFT
CLR	CLEAR, CLEARANCE	LOC	LOCATION
CMP	CORRUGATED METAL PIPE	MH	MANHOLE
CND	CONDUIT	MISC	MISCELLANEOUS
CO	COUNTY, CLEANOUT	MJ	MECHANICAL JOINT
COMM	COMMUNICATIONS	MON	MONUMENT
CONC	CONCRETE	N	NORTH, NORTHING
CONN	CONNECT, CONNECTION	OC	ON CENTER
CONST	CONSTRUCT, CONSTRUCTION	OVHD	OVERHEAD
CONT	CONTINUE, CONTINUOUS	P	PUMP, POWER
CONTR	CONTRACTOR	PC	POINT OF CURVATURE
CORR	CORRUGATED, CORRUGATION	PCC	PORTLAND CEMENT CONCRETE
COS	CITY OF SUMNER	PED	PEDESTAL
CPLG	COUPLING	PI	POINT OF INTERSECTION
CSBC	CRUSHED SURFACING BASE COURSE	POC	POINT ON CURVE
CSTC	CRUSHED SURFACING TOP COURSE	PP	POWER POLE
CTR	CENTER	PT	POINT OF TANGENCY, POINT
CULV	CULVERT	RAD	RADIUS
CV	CONTROL VALVE	RC	REINFORCED CONCRETE
CY	CUBIC YARD	RCP	REINFORCED CONCRETE PIPE
D	DEPTH, DENSITY, DRAIN, DRAINAGE	RD	ROAD, ROOF DRAIN
DEG	DEGREE	RED	REDUCER
DEMO	DEMOLITION	REQD	REQUIRED
DET	DETAIL	ROW	RIGHT OF WAY
DI	DUCTILE IRON	RP	RADIUS POINT
DIA	DIAMETER	RR	RAILROAD
DIM	DIMENSION	RT	RIGHT
DIP	DUCTILE IRON PIPE	SD	STORM DRAIN
DIST	DISTANCE, DISTRICT	SDMH	STORM DRAIN MANHOLE
DMH	DRAINAGE MANHOLE	SIG	SIGNAL
DN	DOWN	SPEC	SPECIFICATION
DSGN	DESIGN	SS	SANITARY SEWER
DWG	DRAWING	SSMH	SANITARY SEWER MANHOLE
E	EAST, EASTING	SW	SIDEWALK
EA	EACH	TBD	TO BE DETERMINED
EC	END OF CURVE	T, TEL	TELEPHONE
EJ	EXPANSION JOINT	TOB	TOP OF BANK
EL	ELEVATION	TOC	TOP OF CONCRETE, TOP OF CURB
ENGR	ENGINEER	TRANS	TRANSPORTATION
EOP	EDGE OF PAVEMENT	TWLT	TWO-WAY LEFT TURN LANE
EQ	EQUAL	TYP	TYPICAL
EQUIP	EQUIPMENT	UP	UTILITY POLE
EQUIV	EQUIVALENT	W	WATER, WATT, WEST, WIDTH
EW	EACH WAY	WSDOT	WASHINGTON STATE DEPARTMENT OF TRANSPORTATION
EX, EXIST	EXISTING	XFMR	TRANSFORMER
EXL	EXCAVATE		

DETAIL AND SECTION DESIGNATION



CONTROL POINTS

POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
PMX #9002	709263.605	1203504.109	66.08	SET PK NAIL IN BIKE TRAIL NORTH SIDE OF 3RD AVE SW
PMX #9006	707888.273	1203571.510	63.15	SET PK NAIL IN ASPHALT AT APPROXIMATE CL OF 5TH AVE SW
PMX #9010	706522.479	1203834.098	61.77	SET PK NAIL IN ASPHALT TRAIL
PMX #9016	705737.644	1203988.792	65.33	SET PK NAIL IN ASPHALT AT THE APPROXIMATE CL OF ROY STREET

HORIZONTAL DATUM

WASHINGTON COORDINATE SYSTEM NAD 83/91, SOUTH ZONE

VERTICAL DATUM

NAVD 88

PRELIMINARY

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NOT FOR CONSTRUCTION**

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

**ONE INCH AT FULL SCALE.
IF NOT, SCALE ACCORDINGLY**
 FILE NAME
 PU3805004P08-G
 JOB No.
 214-3805-004
 DATE
 NOVEMBER 2011

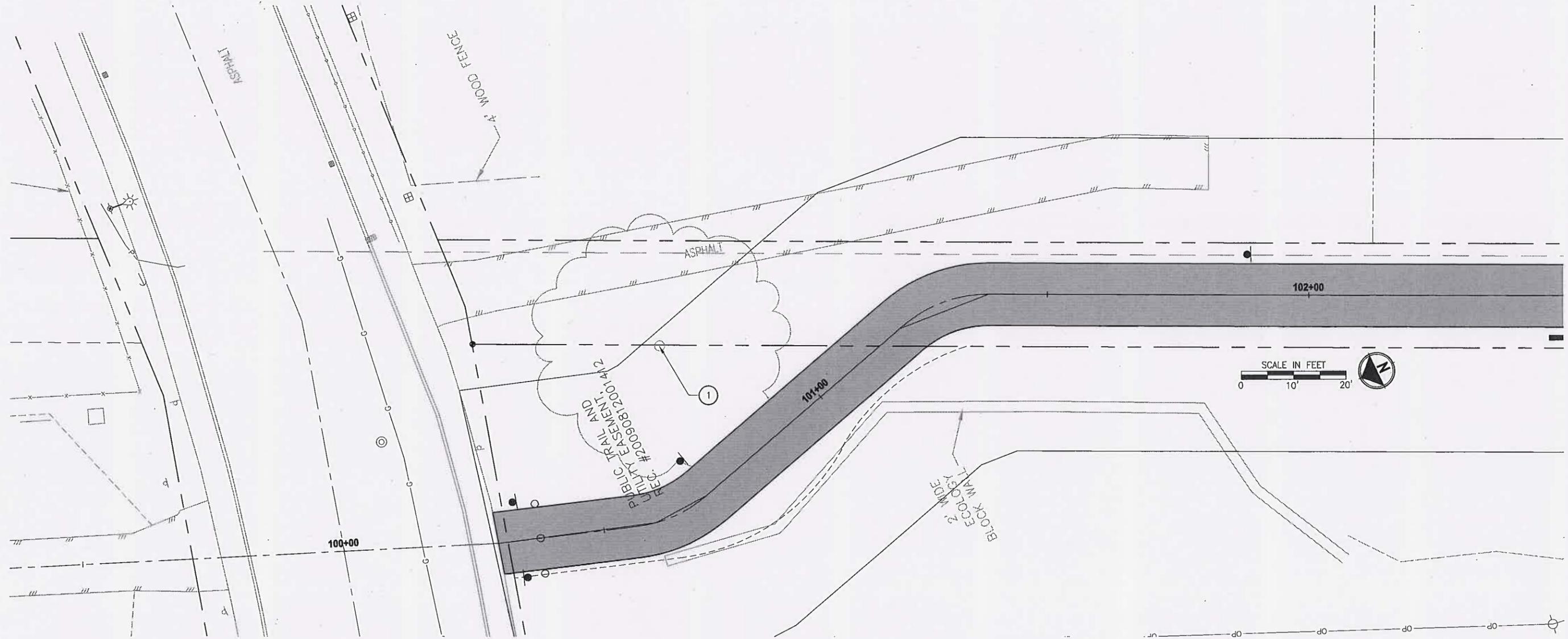
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 1019 39th AVENUE S.E., SUITE 100
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PROJECT NAME
**INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

**LEGEND, NOTES AND
SURVEY CONTROL**

SHEET NO.
 2 OF XX
G2

LAYOUT: DM1
 PATH: U:\Puyallup\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawgs
 PLOTTED BY: stumpnor DATE: Friday, November 11, 2011 3:53:02 PM



TESC NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ALL TESC MEASURES AS IDENTIFIED IN THESE PLANS. ADDITIONAL MEASURES SHALL BE IMPLEMENTED AS NEEDED TO ACCOMMODATE CHANGING WEATHER CONDITION, AS WARRANTED BY CONSTRUCTION METHODS, OR AS REQUIRED PER THE DIRECTION OF THE CITY OR TESC SUPERVISOR AT NO COST TO THE CITY.
2. THE IMPLEMENTATION OF THESE TESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THE TESC MEASURES IS THE RESPONSIBILITY OF THE CONTRACTOR.
3. THE BOUNDARIES OF THE CLEARING LIMITS ARE DEFINED BY THE PROPOSED/EXISTING ROW UNLESS SHOWN OTHERWISE. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE CLEARING LIMITS SHALL BE PERMITTED. THE CLEARING LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR/TESC SUPERVISOR FOR THE DURATION OF CONSTRUCTION.
4. ALL STORM SEWER STRUCTURES SHALL BE PROTECTED WITH A CATCH BASIN INSERT UNTIL PROJECT COMPLETION.

DEMOLITION NOTES:

1. PRIOR TO PLACING PRE-LOADING MATERIAL, ALL GRASS AND BRUSH SHOULD BE CLEARED IN ACCORDANCE WITH THE REQUIREMENTS IN SECTION 2-01.3(1) OF THE 2010 WASHINGTON STANDARD SPECIFICATIONS PER THE GEOTECHNICAL REPORT COMPLETED BY LANDAU, JUNE 2011.
2. DO NOT REMOVE THE ROOT AND TOPSOIL MAT ALONG THE FULL DURATION OF THE PROPOSED TRAIL LIMITS PER THE GEOTECHNICAL REPORT COMPLETED BY LANDAU, JUNE 2011.
3. USE THE ABOVE MENTIONED CLEARING AND GRUBBING TECHNIQUES FOR THE FULL LENGTH OF THE PROPOSED TRAIL UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

GENERAL NOTES:

1. PROTECT TREE.

PRELIMINARY

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REVISIONS	DATE	BY	DESIGNED
			M.STUMPF
			DRAWN
			M.STUMPF
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			APPROVED

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 JOB No.: 214-3805-004
 DATE: NOVEMBER 2011

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PROJECT NAME
**INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

**DEMOLITION AND TESC
 PLAN FOR ELECTRIC AVE S**

SHEET NO.
3 OF XX

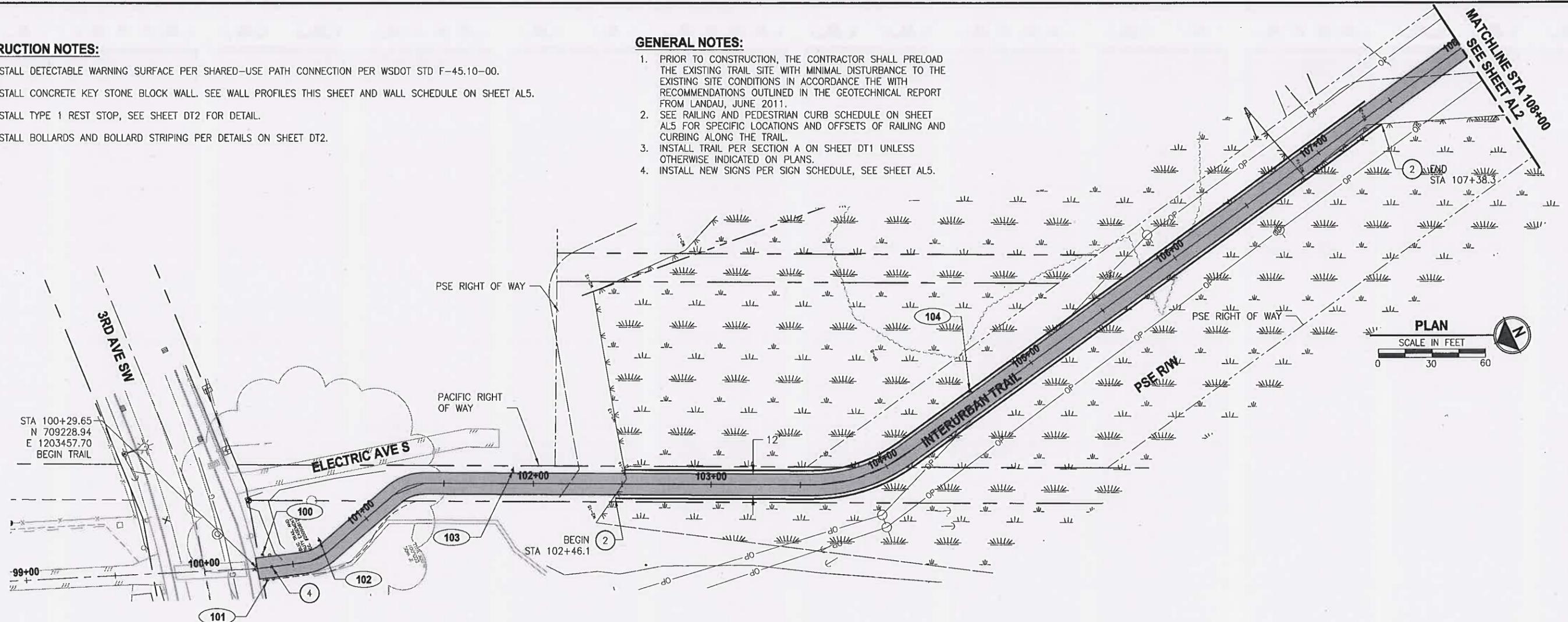
DM1

CONSTRUCTION NOTES:

1. INSTALL DETECTABLE WARNING SURFACE PER SHARED-USE PATH CONNECTION PER WSDOT STD F-45.10-00.
2. INSTALL CONCRETE KEY STONE BLOCK WALL. SEE WALL PROFILES THIS SHEET AND WALL SCHEDULE ON SHEET AL5.
3. INSTALL TYPE 1 REST STOP, SEE SHEET DT2 FOR DETAIL.
4. INSTALL BOLLARDS AND BOLLARD STRIPING PER DETAILS ON SHEET DT2.

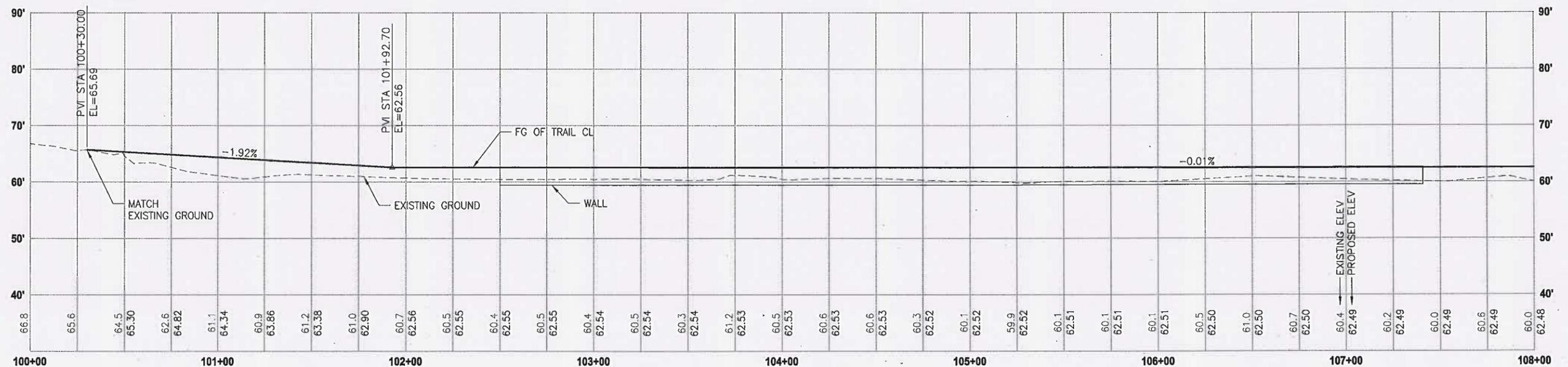
GENERAL NOTES:

1. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL PRELOAD THE EXISTING TRAIL SITE WITH MINIMAL DISTURBANCE TO THE EXISTING SITE CONDITIONS IN ACCORDANCE WITH THE RECOMMENDATIONS OUTLINED IN THE GEOTECHNICAL REPORT FROM LANDAU, JUNE 2011.
2. SEE RAILING AND PEDESTRIAN CURB SCHEDULE ON SHEET AL5 FOR SPECIFIC LOCATIONS AND OFFSETS OF RAILING AND CURBING ALONG THE TRAIL.
3. INSTALL TRAIL PER SECTION A ON SHEET DT1 UNLESS OTHERWISE INDICATED ON PLANS.
4. INSTALL NEW SIGNS PER SIGN SCHEDULE, SEE SHEET AL5.



LEGEND:

- XXX SIGN TAG
- WALL
- WETLANDS
- DETECTABLE RAMP



PROFILE

HORIZ: 1"=30'
VERT: 1"=5'

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LAYOUT: AL1 PATH: U:\paya\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Draws\

PRELIMINARY

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			M. STUMPF
			CHECKED
			APPROVED

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 FILE NAME: PU3805004P08-AL
 JOB No: 214-3805-004
 DATE: NOVEMBER 2011

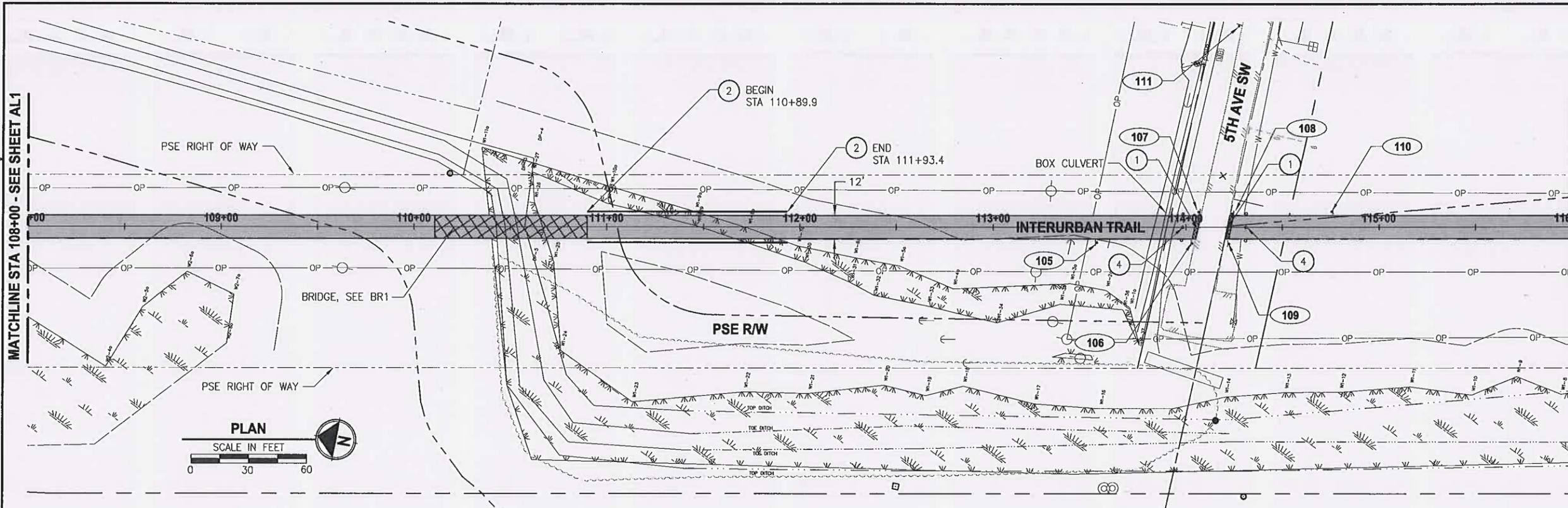
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PROJECT NAME
 INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD
 PACIFIC, WASHINGTON

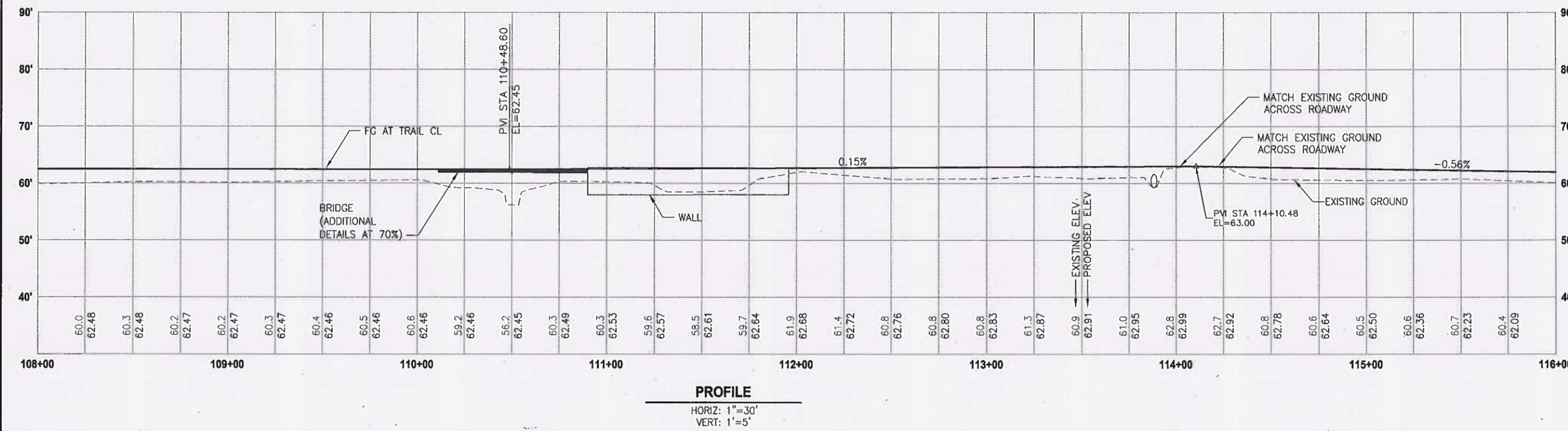
**ELECTRIC AVE TRAIL GRADING
 ALIGNMENT, AND SIGNAGE PLAN
 STA 100+00 TO STA 108+00**

SHEET NO.
 4 OF XX
AL1

LAYOUT: AL2 PATH: U:\Puyo\Projects\Clients\3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawn PLOTTED BY: stumpfor DATE: Friday, November 11, 2011 5:09:58 PM



- CONSTRUCTION NOTES:**
1. INSTALL DETECTABLE WARNING SURFACE PER SHARED-USE PATH CONNECTION PER WSDOT STD F-45.10-00.
 2. INSTALL CONCRETE KEY STONE BLOCK WALL. SEE WALL PROFILES THIS SHEET AND WALL SCHEDULE ON SHEET AL5.
 3. INSTALL TYPE 1 REST STOP, SEE SHEET DT2 FOR DETAIL.
 4. INSTALL BOLLARDS AND BOLLARD STRIPING PER DETAILS ON SHEET DT2.



- GENERAL NOTES:**
1. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL PRELOAD THE EXISTING TRAIL SITE WITH MINIMAL DISTURBANCE TO THE EXISTING SITE CONDITIONS IN ACCORDANCE WITH THE RECOMMENDATIONS OUTLINED IN THE GEOTECHNICAL REPORT FROM LANDAU, JUNE 2011.
 2. SEE RAILING AND PEDESTRIAN CURB SCHEDULE ON SHEET AL5 FOR SPECIFIC LOCATIONS AND OFFSETS OF RAILING AND CURBING ALONG THE TRAIL.
 3. INSTALL TRAIL PER SECTION A ON SHEET DT1 UNLESS OTHERWISE INDICATED ON PLANS.
 4. INSTALL NEW SIGNS PER SIGN SCHEDULE, SEE SHEET AL5.

- LEGEND:**
- XXX SIGN TAG
 - WALL
 - - - WETLANDS
 - DETECTABLE RAMP

**50 % REVIEW SUBMITTAL
NOT FOR CONSTRUCTION**

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

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FILE NAME: PU3805004P08-AL
JOB No: 214-3805-004
DATE: NOVEMBER 2011

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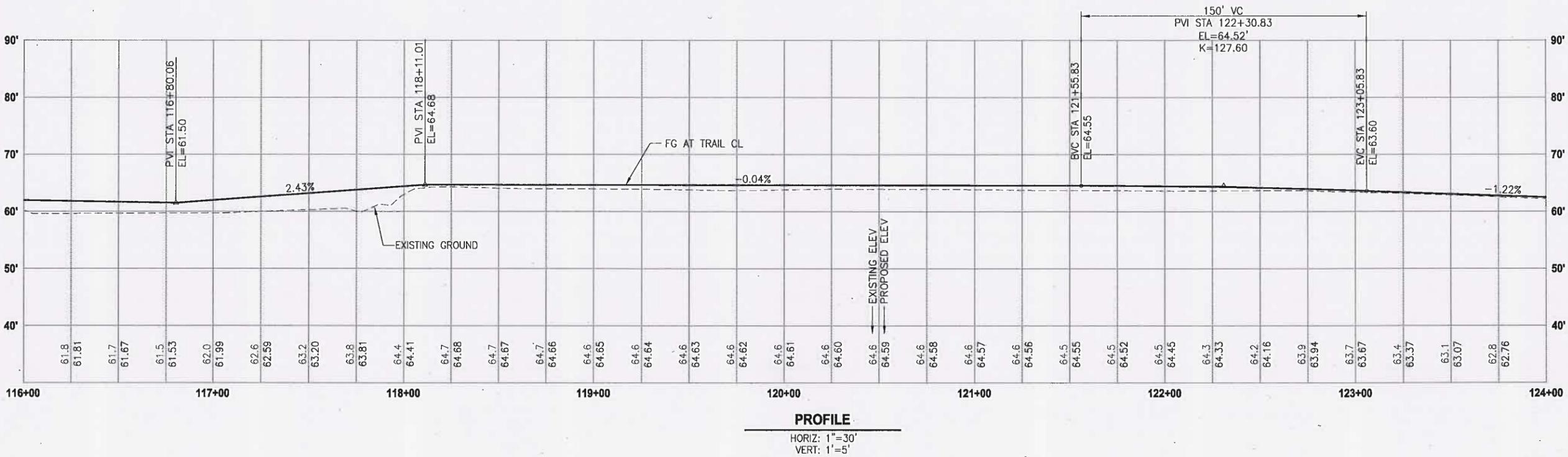
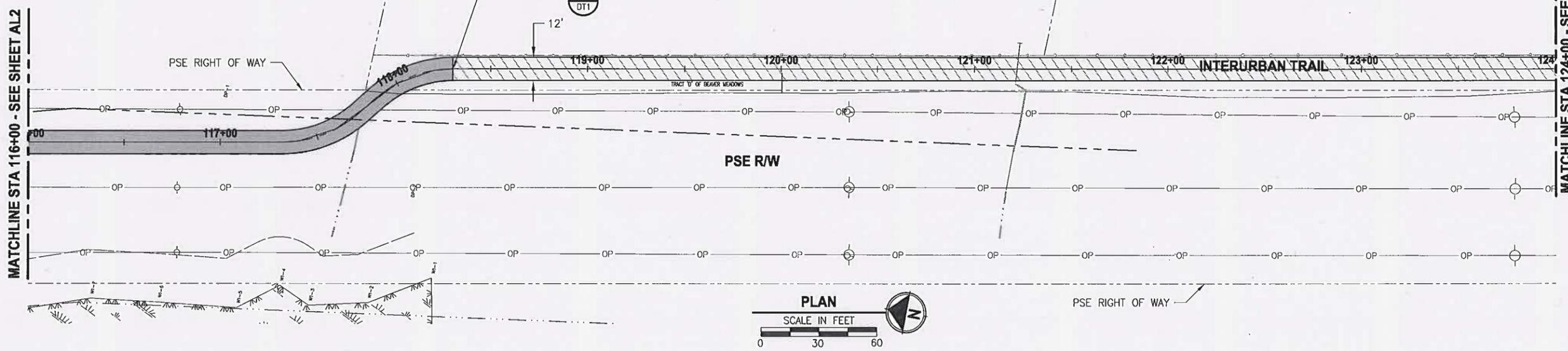
1019 39th AVENUE S.E., SUITE 100
PUYALLUP, WASHINGTON 98374
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PROJECT NAME
**INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD**
PACIFIC, WASHINGTON

**PSE TRAIL GRADING,
ALIGNMENT AND SIGNAGE PLAN
STA 108+00 TO STA 116+00**

SHEET NO.
5 OF XX
AL2

LAYOUT: AL3
 PATH: U:\P\p\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawn\PLOTTED BY: slumpner DATE: Friday, November 11, 2011 5:10:36 PM



- CONSTRUCTION NOTES:**
1. INSTALL DETECTABLE WARNING SURFACE PER SHARED-USE PATH CONNECTION PER WSDOT STD F-45.10-00.
 2. INSTALL CONCRETE KEY STONE BLOCK WALL. SEE WALL PROFILES THIS SHEET AND WALL SCHEDULE ON SHEET AL5.
 3. INSTALL TYPE 1 REST STOP, SEE SHEET DT2 FOR DETAIL.
 4. INSTALL BOLLARDS AND BOLLARD STRIPING PER DETAILS ON SHEET DT2.

- GENERAL NOTES:**
1. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL PRELOAD THE EXISTING TRAIL SITE WITH MINIMAL DISTURBANCE TO THE EXISTING SITE CONDITIONS IN ACCORDANCE WITH THE RECOMMENDATIONS OUTLINED IN THE GEOTECHNICAL REPORT FROM LANDAU, JUNE 2011.
 2. SEE RAILING AND PEDESTRIAN CURB SCHEDULE ON SHEET AL5 FOR SPECIFIC LOCATIONS AND OFFSETS OF RAILING AND CURBING ALONG THE TRAIL.
 3. INSTALL TRAIL PER SECTION A ON SHEET DT1 UNLESS OTHERWISE INDICATED ON PLANS.
 4. INSTALL NEW SIGNS PER SIGN SCHEDULE, SEE SHEET AL5.

- LEGEND:**
- XXX SIGN TAG
 - WALL
 - WETLANDS
 - DETECTABLE RAMP

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NOT FOR CONSTRUCTION**

REVISIONS	DATE	BY	DESIGNED
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			M. STUMPF
			CHECKED
			APPROVED

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IF NOT, SCALE ACCORDINGLY**
 FILE NAME
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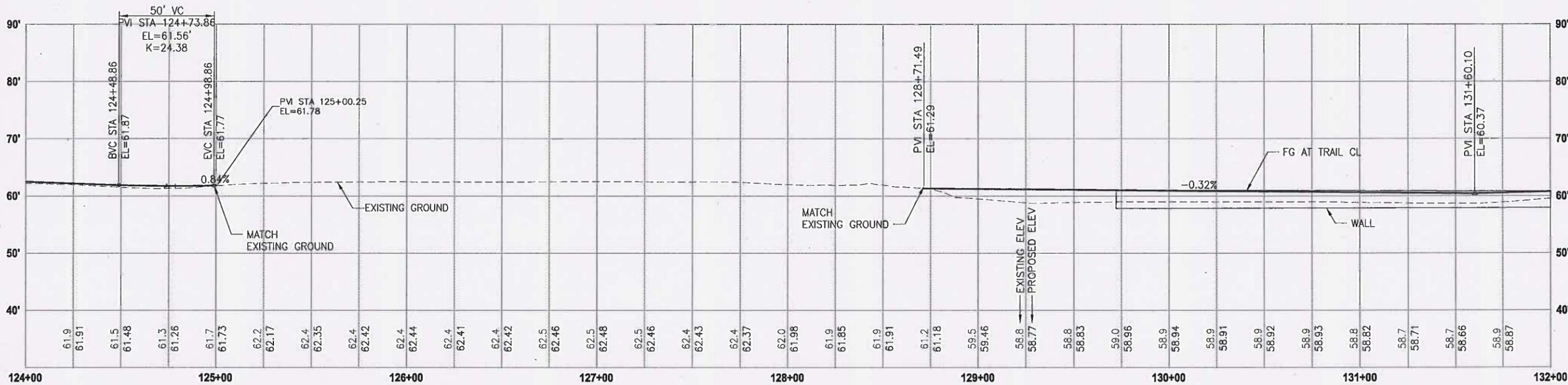
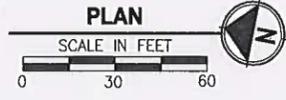
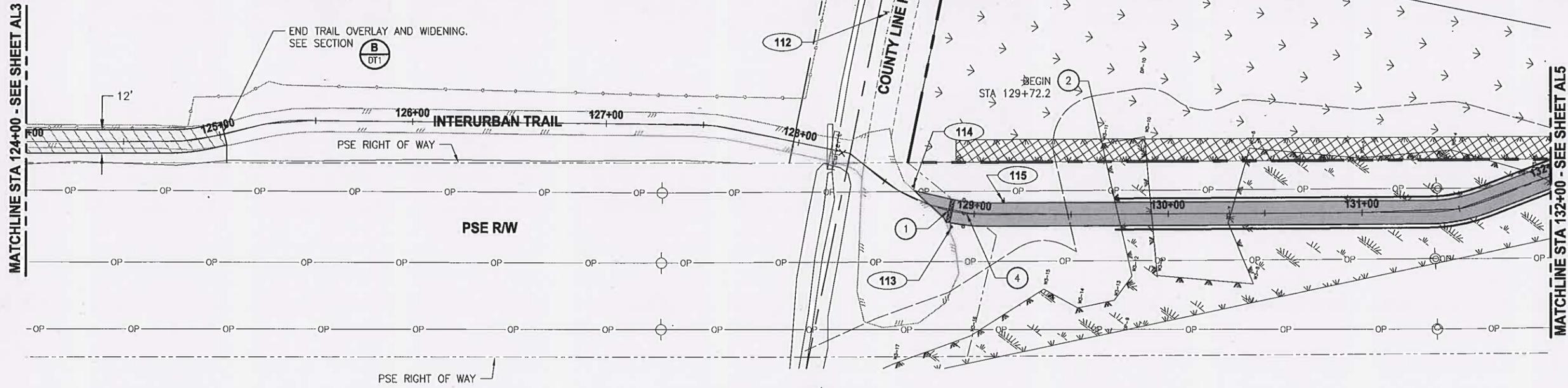
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 RUYAL LLP, WASHINGTON 98374
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PROJECT NAME
**INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

**PSE TRAIL GRADING
ALIGNMENT AND SIGNAGE PLAN
STA 116+00 TO STA 124+00**

SHEET NO.
 6 OF XX
AL3

LAYOUT: AL4
 PATH: U:\P\proj\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawn\ PLOTTED BY: stumpfor DATE: Friday, November 11, 2011 5:10:47 PM



PROFILE
 HORIZ: 1"=30'
 VERT: 1'=5'

CONSTRUCTION NOTES:

1. INSTALL DETECTABLE WARNING SURFACE PER SHARED-USE PATH CONNECTION PER WSDOT STD F-45.10-00.
2. INSTALL CONCRETE KEY STONE BLOCK WALL. SEE WALL PROFILES THIS SHEET AND WALL SCHEDULE ON SHEET AL5.
3. INSTALL TYPE 1 REST STOP, SEE SHEET DT2 FOR DETAIL.
4. INSTALL BOLLARDS AND BOLLARD STRIPING PER DETAILS ON SHEET DT2.

GENERAL NOTES:

1. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL PRELOAD THE EXISTING TRAIL SITE WITH MINIMAL DISTURBANCE TO THE EXISTING SITE CONDITIONS IN ACCORDANCE WITH THE RECOMMENDATIONS OUTLINED IN THE GEOTECHNICAL REPORT FROM LANDAU, JUNE 2011.
2. SEE RAILING AND PEDESTRIAN CURB SCHEDULE ON SHEET AL5 FOR SPECIFIC LOCATIONS AND OFFSETS OF RAILING AND CURBING ALONG THE TRAIL.
3. INSTALL TRAIL PER SECTION A ON SHEET DT1 UNLESS OTHERWISE INDICATED ON PLANS.
4. INSTALL NEW SIGNS PER SIGN SCHEDULE, SEE SHEET AL5.

LEGEND:

- XXX SIGN TAG
- WALL
- WETLANDS
- DETECTABLE RAMP

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REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
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			M. STUMPF
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 FILE NAME: PU3805004P08-AL
 JOB No: 214-3805-004
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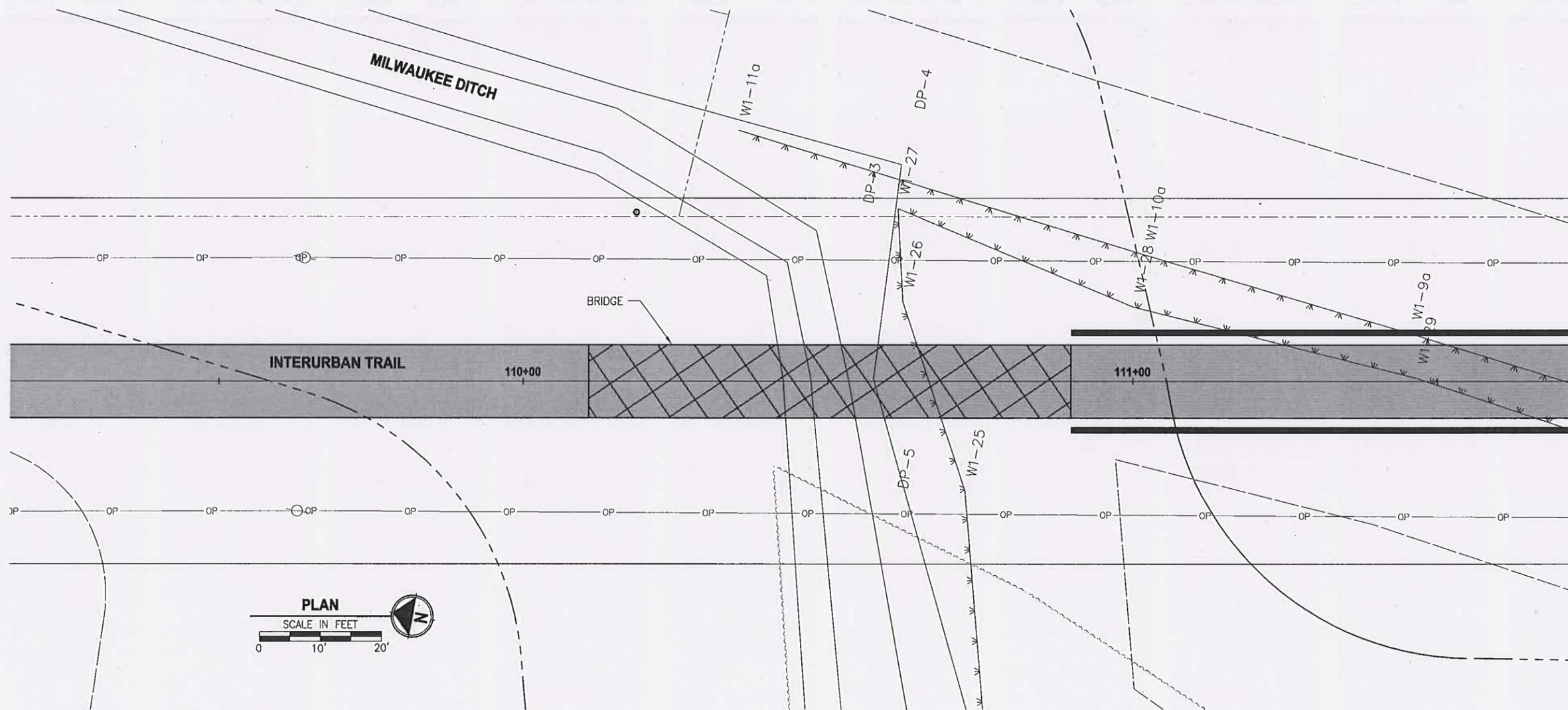
PROJECT NAME
**INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

**PSE TRAIL GRADING
 ALIGNMENT AND SIGNAGE PLAN
 STA 124+00 TO STA 132+00**

SHEET NO.
 7 OF XX
AL4

CONSTRUCTION NOTES:

NOTES AND DETAILS TO BE COMPLETED BY 70%.



LAYOUT: BR1
 PATH: U:\Puyo\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawg\

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 FILE NAME: PU3805004P08-DT2
 JOB No. 214-3805-004
 DATE: NOVEMBER 2011

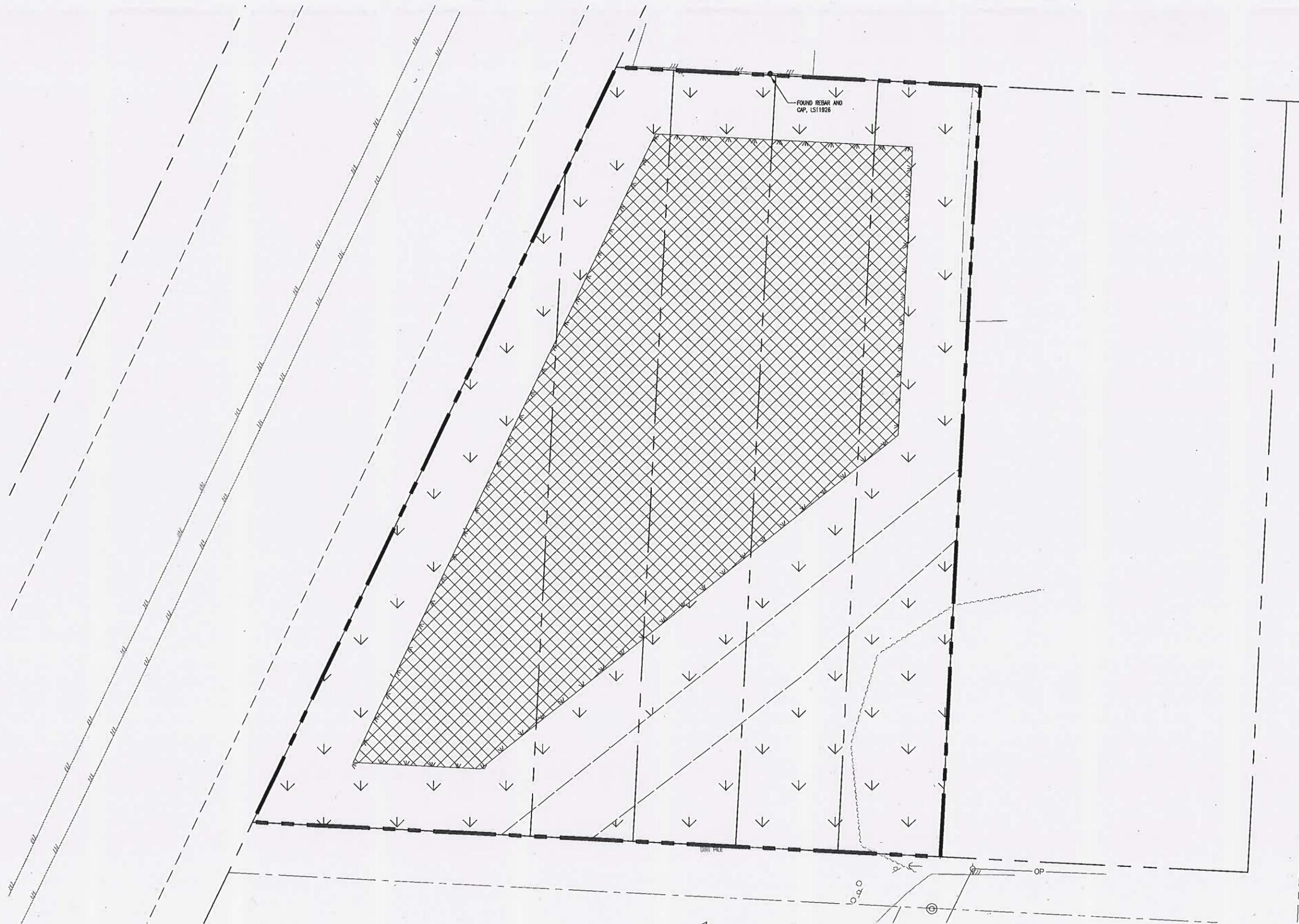
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PROJECT NAME
INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD
 PACIFIC, WASHINGTON

MILWAUKEE DITCH BRIDGE

SHEET NO.
 9 OF XX
BR1

PATH: U:\Puyo\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Draws\ PLOTTED BY: stumpfer DATE: Friday, November 11, 2011 3:58:51 PM
 LAYOUT: WT1



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 JOB No.
 214-3805-004
 DATE
 NOVEMBER 2011

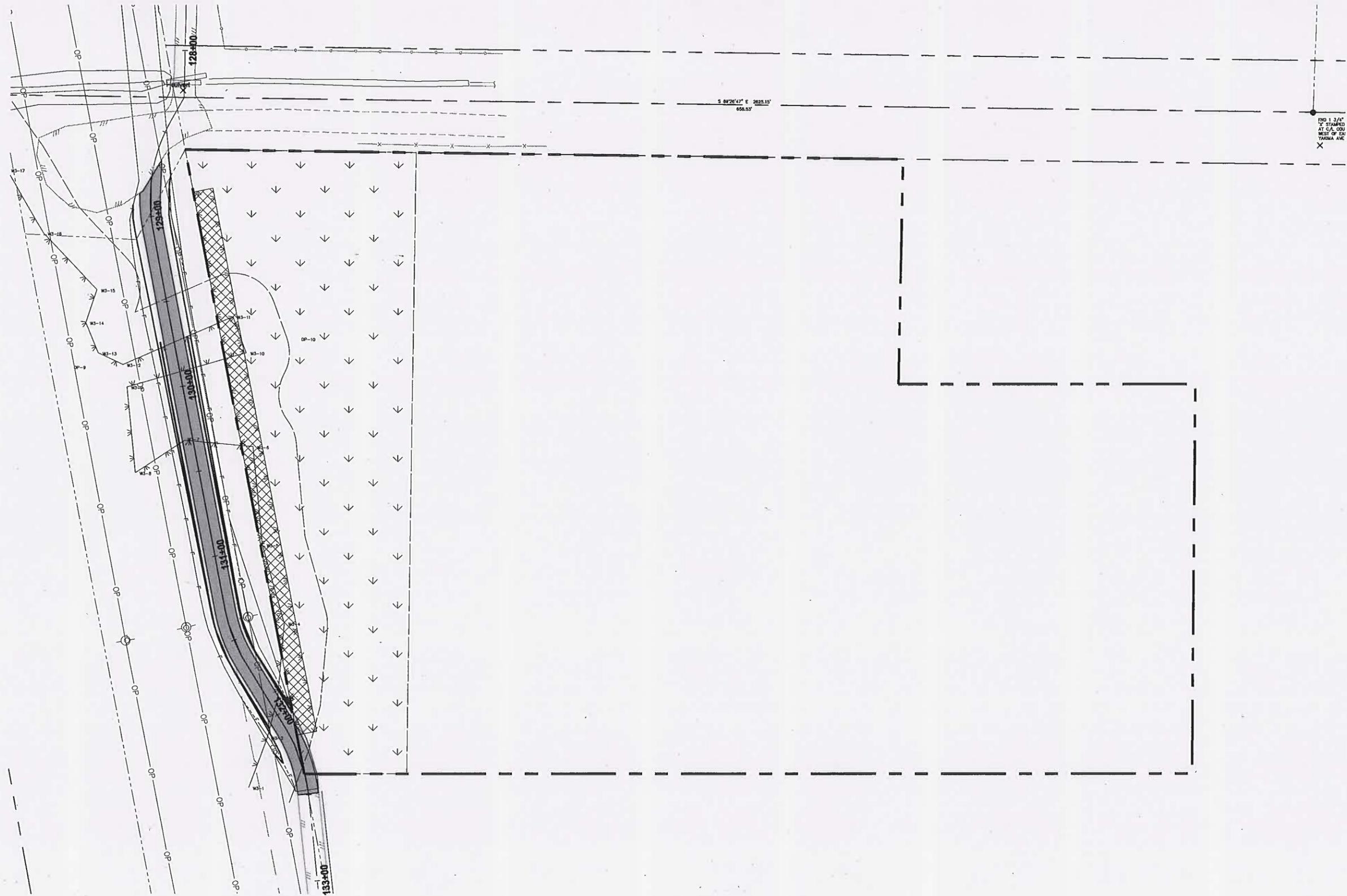
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PROJECT NAME
 INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD
 PACIFIC, WASHINGTON

FINAL WETLAND
 MITIGATION PLAN

SHEET NO.
 10 OF XX
 WT1

PATH: U:\P\p\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Draws\ PLOTTED BY: stumpnor DATE: Friday, November 11, 2011 3:59:54 PM LAYOUT: WT2



END 1/4" X STAMPED AT 0.1 0.00 WEST OF EA TARRA AVE X

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PRELIMINARY

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			M.STUMPF
			CHECKED
			APPROVED

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FILE NAME
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JOB No.
214-3805-004
DATE
NOVEMBER 2011

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1919 308 AVENUE S.E., SUITE 100
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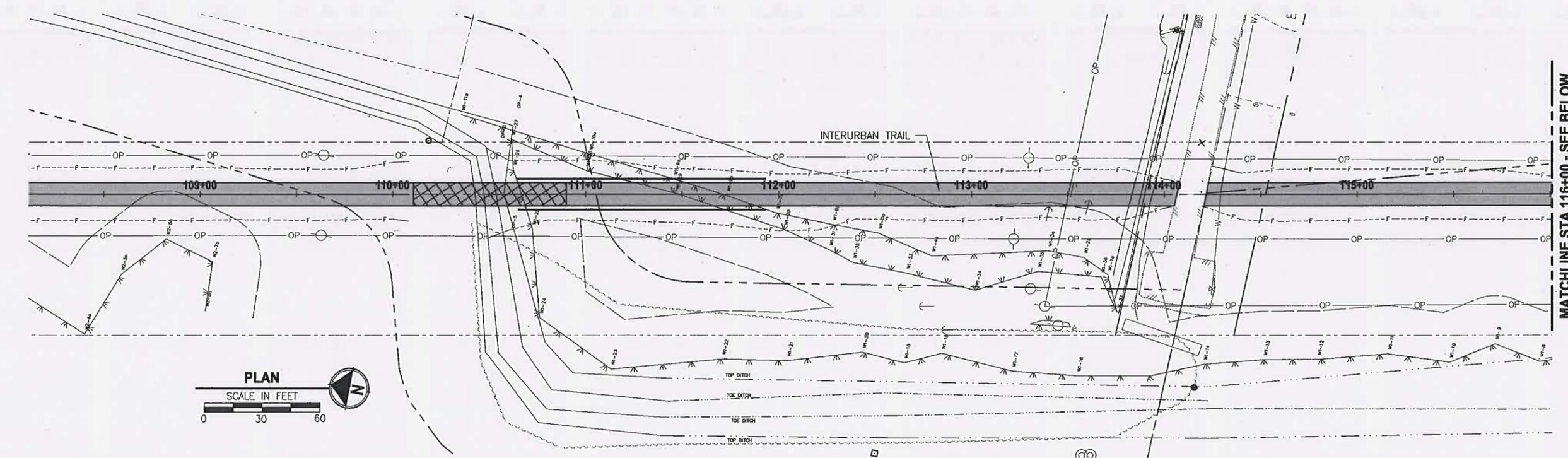
PROJECT NAME
INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD
PACIFIC, WASHINGTON

FINAL WETLAND
MITIGATION PLAN

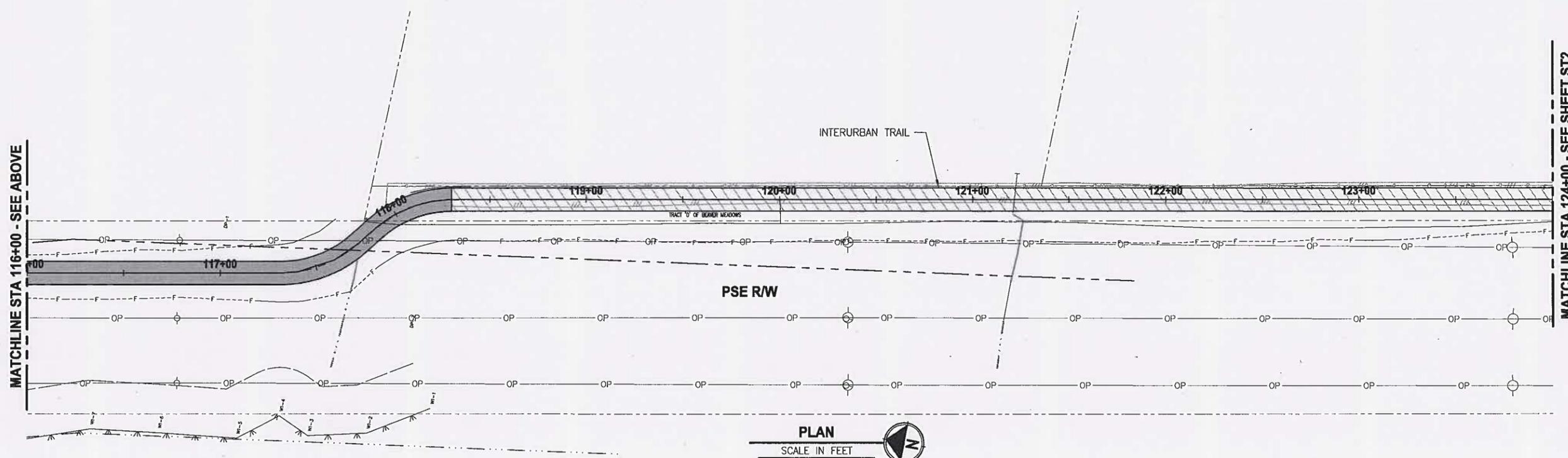
SHEET NO.
11 OF XX
WT2

LAYOUT: ST1
 PATH: U:\Puyo\Projects\Clients\3805-City of Pacific\214-3805-04-Interurban Trail\CAD\Phase 08 PSE Corridor\Draws\
 PLOTTED BY: atumpmor DATE: Friday, November 11, 2011 4:02:48 PM

CONSTRUCTION NOTES:



MATCHLINE STA 116+00 - SEE BELOW



MATCHLINE STA 124+00 - SEE SHEET ST2

MATCHLINE STA 116+00 - SEE ABOVE

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			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

**ONE INCH AT FULL SCALE,
IF NOT, SCALE ACCORDINGLY**
 FILE NAME
 PU3805004P08-WT
 JOB No.
 214-3805-004
 DATE
 NOVEMBER 2011

PRELIMINARY

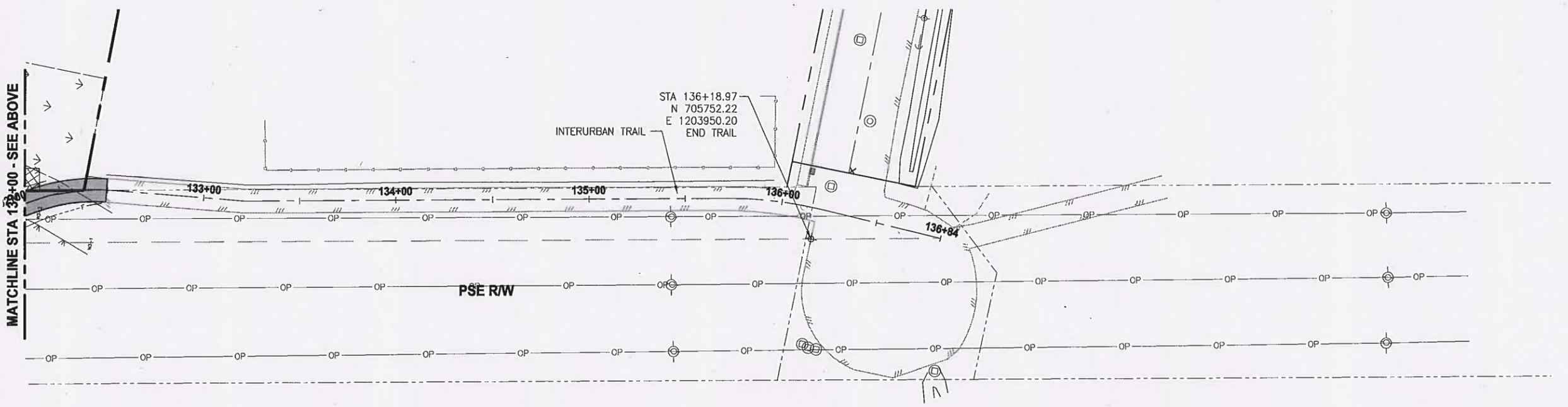
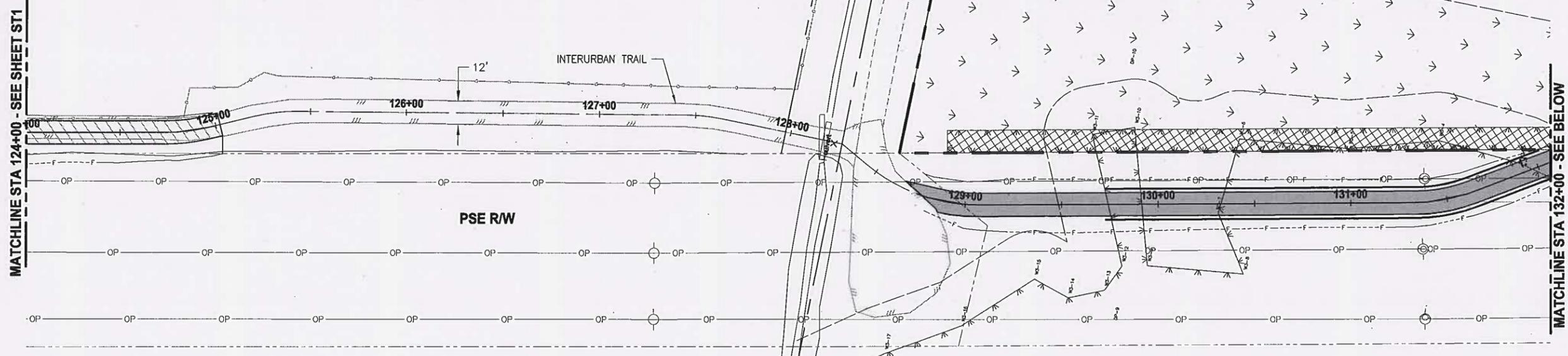
Parametrix
 ENGINEERING . PLANNING . ENVIRONMENTAL SCIENCES
 1019 30th AVENUE S.E., SUITE 100
 PUYALLUP, WASHINGTON 98374
 T. 253.604.6600 F. 253.604.6759
 www.parametrix.com

PROJECT NAME
**INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

**FINAL STREAM BUFFER
RESTORATION PLANTING PLAN**

SHEET NO.
 12 OF XX
ST1

CONSTRUCTION NOTES:



PATH: U:\p\p\p\Projects\Clients\3805-City of Pacific\214-3805-04-Interurban Trail\CAD\Phase 08 PSE Corridor\Drawg\ PLOTTED BY: slumpmor DATE: Friday, November 11, 2011 3:57:46 PM
 LAYOUT: ST2

PRELIMINARY

50 % REVIEW SUBMITTAL
NOT FOR CONSTRUCTION

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

**ONE INCH AT FULL SCALE,
IF NOT, SCALE ACCORDINGLY**
 FILE NAME:
 PU3805004P08-WT
 JOB No.
 214-3805-004
 DATE
 NOVEMBER 2011

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 10119 39th AVENUE S.E., SUITE 100
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 T. 253.604.6600 F. 253.604.6799
 www.parametrix.com

PROJECT NAME
**INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

**FINAL STREAM BUFFER
 RESTORATION PLANTING PLAN**

SHEET NO.
 13 OF XX
ST2

LAYOUT: LSI PATH: u:\paya\Projects\Clients\3805-City of Pacific\214-3805-004-Interurban Trail\CAD\Phase 08 PSE Corridor\Draws\ PLOTTED BY: stumpmer DATE: Friday, November 11, 2011 4:03:50 PM

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			M. STUMPF
			CHECKED
			APPROVED

ONE INCH AT FULL SCALE. IF NOT, SCALE ACCORDINGLY.	
FILE NAME	PU3805004P08-WT
JOB No.	214-3805-004
DATE	NOVEMBER 2011

PRELIMINARY

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1019 39th AVENUE S.E. SUITE 100 PUYALLUP, WASHINGTON 98374 T. 253.604.6900 F. 253.604.6799 www.parametrix.com	

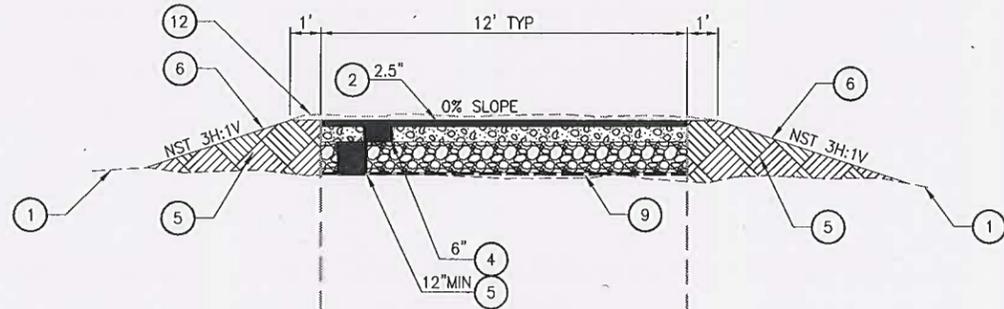
PROJECT NAME	INTERURBAN TRAIL EXTENSION PSE/STEWART ROAD CORRIDOR 3RD AVENUE SW TO ROY ROAD PACIFIC, WASHINGTON
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PLANTING NOTES AND DETAILS

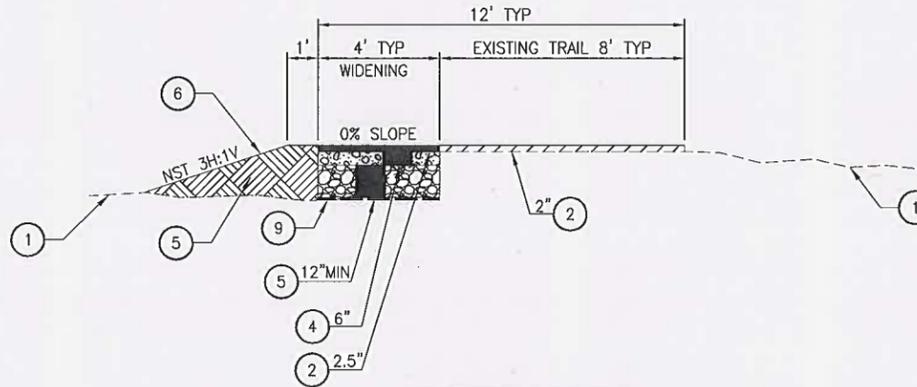
SHEET NO. 14 OF XX
LS1

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APPROXIMATE LOCATION(S)
 STA 101+35 TO STA 118+30
 STA 118+30 TO STA 132+47
TYPICAL SECTION **A**
 NO SCALE



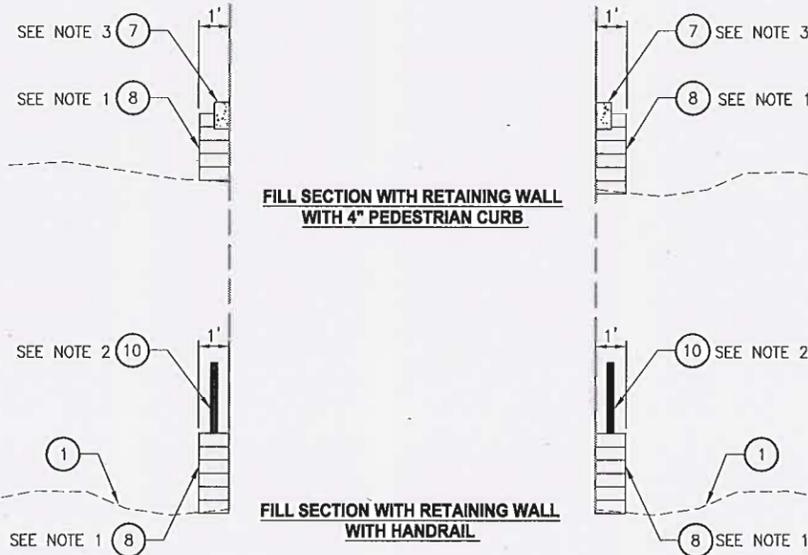
APPROXIMATE LOCATION(S)
 STA 118+30 TO STA 125+00
OVERLAY SECTION **B**
 NO SCALE

LEGEND:

- 1 EXISTING GROUND
- 2 POROUS ASPHALT CONCRETE.
- 3 NOT USED.
- 4 RESERVOIR ROCK.
- 5 STRUCTURAL FILL.
- 6 INSTALL PLANTINGS AND LANDSCAPING PER WETLAND MITIGATION PLANS AND LANDSCAPING PLANS.
- 7 CEMENT CONCRETE PEDESTRIAN CURB PER WSDOT STD DETAIL F-10.12-01.
- 8 KEYSTONE BLOCK WALL.
- 9 WOVEN GEOTEXTILE.
- 10 HANDRAIL
- 11 2" HMA OVERLAY
- 12 PRELOAD MATERIAL. REMOVAL OR ADDITIONAL MATERIAL MAY BE REQUIRED TO MEET THE FINISH GRADE PROFILE DUE TO IRREGULAR GROUND SETTLEMENT.

GENERAL NOTES:

1. EXACT WALL LOCATIONS VARY THROUGHOUT SECTION. SEE TRAIL GRADING AND ALIGNMENT PLANS FOR MORE INFORMATION.
2. INSTALL HANDRAIL (42" MINIMUM HEIGHT) WHERE SPECIFIED IN THE TRAIL GRADING AND ALIGNMENT PLANS, TYPICALLY WHERE THE VERTICAL DROP IS GREATER THAN 2'6". SEE RAILING/CURB SCHEDULE ON SHEET AL5 FOR MORE INFORMATION.
3. INSTALL 4" PEDESTRIAN CURB AT BACK OF TRAIL WHERE VERTICAL WALLS ARE PRESENT, TYPICALLY WHERE THE VERTICAL DROP IS LESS THAN 2'6". SEE RAILING/CURB SCHEDULE ON SHEET AL5 FOR MORE INFORMATION.



**FILL SECTION WITH RETAINING WALL
 WITH 4" PEDESTRIAN CURB**

**FILL SECTION WITH RETAINING WALL
 WITH HANDRAIL**

PRELIMINARY

**50 % REVIEW SUBMITTAL
 NOT FOR CONSTRUCTION**

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

**ONE INCH AT FULL SCALE.
 IF NOT, SCALE ACCORDINGLY**
 FILE NAME: PU3805004P08-DT
 JOB No: 214-3805-004
 DATE: NOVEMBER 2011

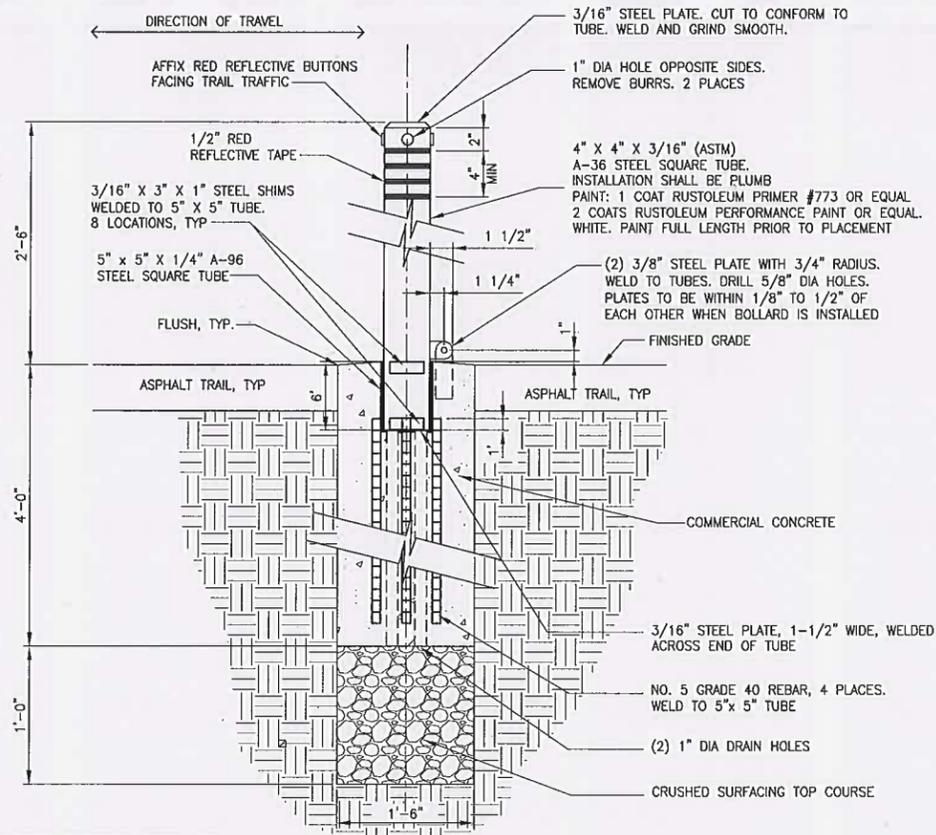
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 PUYALLUP, WASHINGTON 98374
 T. 253.004.6900 F. 253.004.6799
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PROJECT NAME
**INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

TRAIL SECTIONS

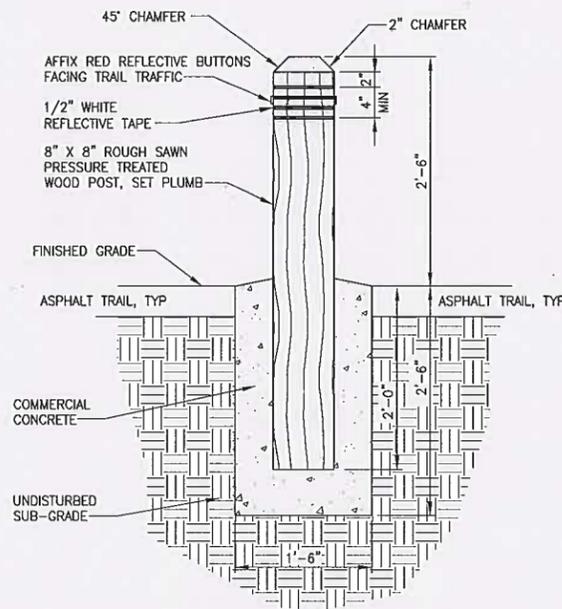
SHEET NO.
 15 OF XX
DT1

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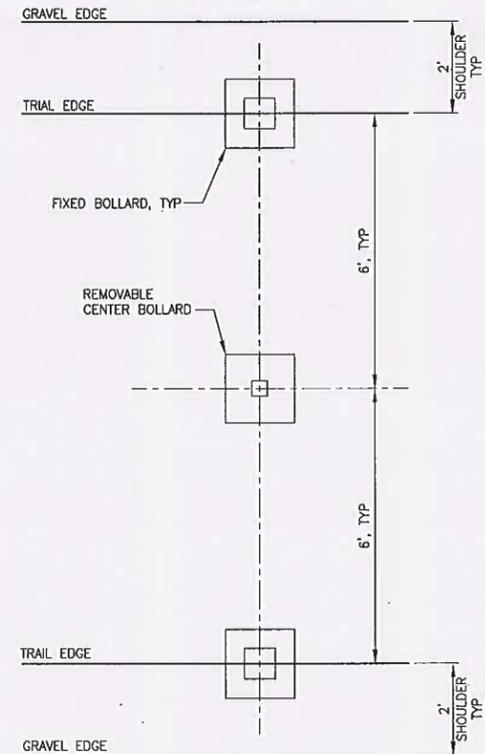


**REMOVABLE BOLLARD
DETAIL**
NOT TO SCALE

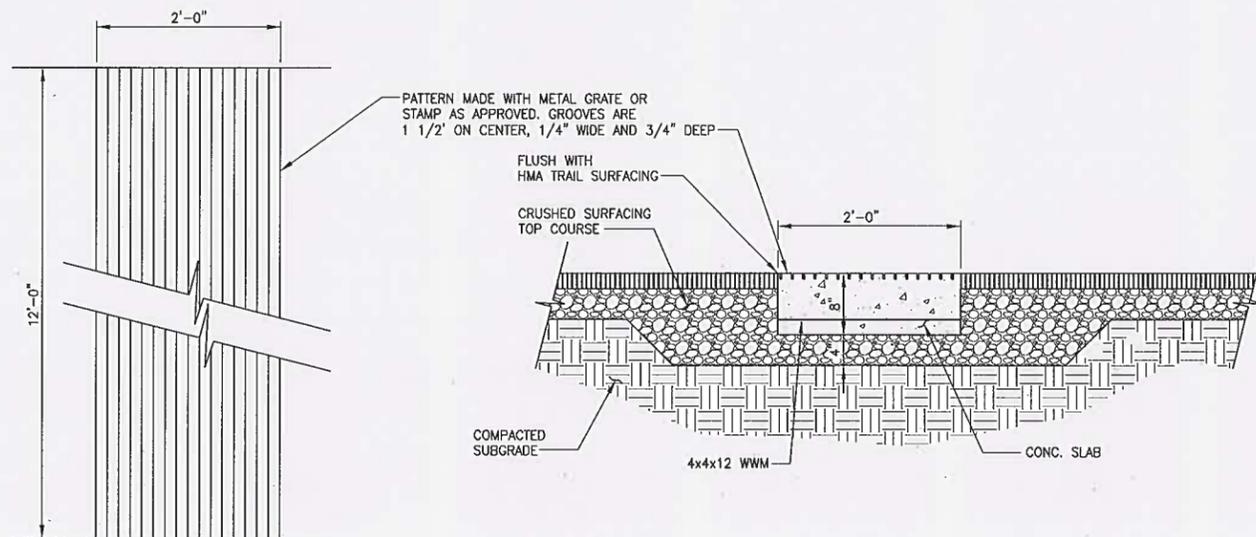
- NOTES:**
- PADLOCK SHALL BE MASTER KEYED ALIKE TOSM-737 WITH NO. 5LF SHACKLE, OWNER PROVIDED.
 - SLOPE TOP OF CONCRETE FOOTING TO DRAIN AWAY FROM POST.
 - HOT DIP GALV. & PRIME PAINT - FINISH COLOR TO BE WHITE MARINE ENAMEL.



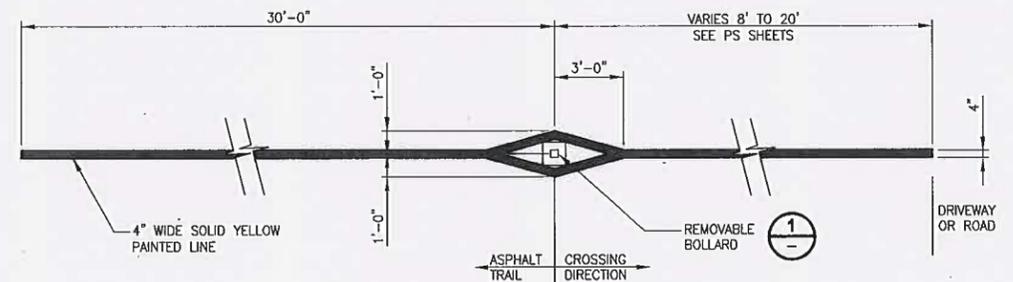
**FIXED BOLLARD
DETAIL**
NOT TO SCALE



**BOLLARD PLAN LOCATION
DETAIL**
NOT TO SCALE



**CONCRETE WARNING BAND
DETAIL**
NOT TO SCALE



**BOLLARD STRIPING
DETAIL**
NOT TO SCALE

**50% REVIEW SUBMITTAL
NOT FOR CONSTRUCTION**

REVISIONS	DATE	BY	DESIGNED
			M. STUMPF
			DRAWN
			M. STUMPF
			CHECKED
			APPROVED

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IF NOT, SCALE ACCORDINGLY**
 FILE NAME: PU3805004P08-DT
 JOB No: 214-3805-004
 DATE: NOVEMBER 2011

PRELIMINARY

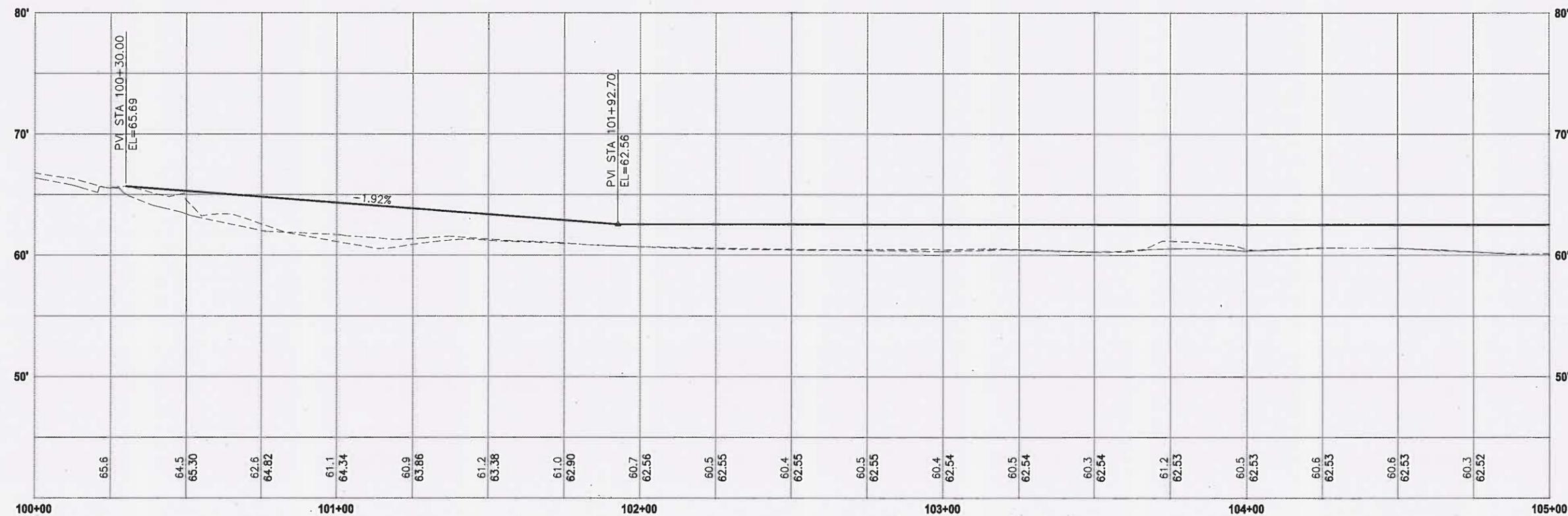
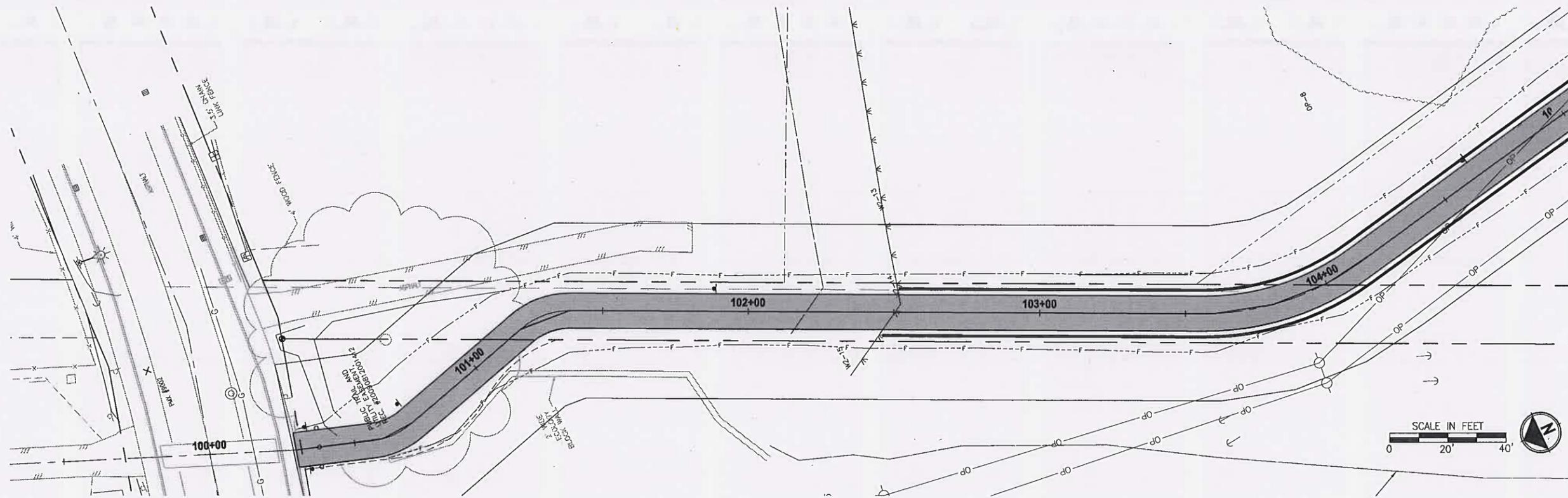
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 1019 39th AVENUE S.E., SUITE 100
 PUYALLUP, WASHINGTON 98374
 T: 253.604.9000 F: 253.604.9799
 www.parametrix.com

PROJECT NAME
**INTERURBAN TRAIL EXTENSION
 PSE/STEWART ROAD CORRIDOR
 3RD AVENUE SW TO ROY ROAD**
 PACIFIC, WASHINGTON

TYPICAL DETAILS

SHEET NO.
 17 OF XX
DT3

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PRELIMINARY

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			M. STUMPF
			CHECKED
			APPROVED

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FILE NAME
PU3805004P08-G
JOB No.
214-3805-004
DATE
NOVEMBER 2011

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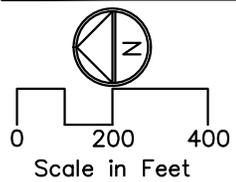
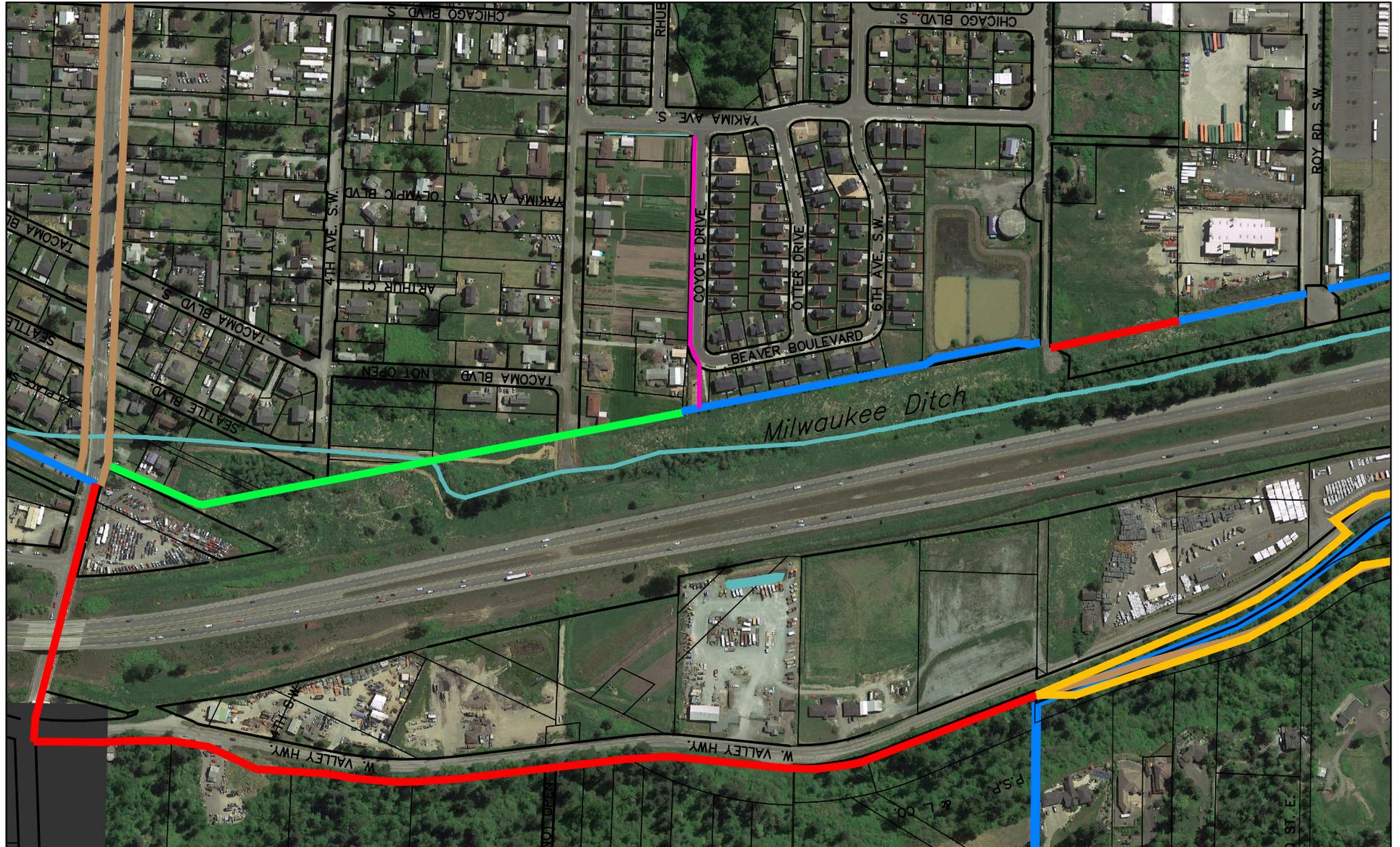
PROJECT NAME
**INTERURBAN TRAIL EXTENSION
PSE/STEWART ROAD CORRIDOR
3RD AVENUE SW TO ROY ROAD**
PACIFIC, WASHINGTON

**STORMWATER
CONVEYANCE PLAN**

SHEET NO.
18 OF XX
SD1

**PSE Trail
50% Design
Construction Cost Estimate**

ITEM NO.	DESCRIPTION	QTY	UNIT	UNIT PRICE	TOTAL
1	Minor Change	1	EST	\$5,000.00	\$5,000.00
2	Roadway Surveying	1	LS	\$12,000.00	\$12,000.00
3	SPCC Plan	1	LS	\$2,500.00	\$2,500.00
4	Mobilization	1	LS	\$68,907.50	\$68,907.50
5	Project Temporary Traffic Control	1	LS	\$1,500.00	\$1,500.00
6	Clearing and Grubbing	1	LS	\$20,000.00	\$20,000.00
7	Roadside Cleanup	1	FA	\$1,500.00	\$1,500.00
8	Potholing	1	FA	\$500.00	\$500.00
9	Removal of Structure and Obstructions	1	LS	\$2,500.00	\$2,500.00
10	Structural Fill	3,950	Ton	\$20.00	\$79,000.00
11	Reservoir Rock	980	Ton	\$30.00	\$29,400.00
12	Woven Geotextile	3,175	SY	\$3.00	\$9,525.00
13	ESC Lead	60	Day	\$120.00	\$7,200.00
14	Silt Fence	5,660	LF	\$5.00	\$28,300.00
15	Erosion/Water Pollution Control	1	FA	\$3,000.00	\$3,000.00
16	Seeding, Fertilizing, and Mulching	0.50	Acre	\$5,000.00	\$2,500.00
17	Wetland Mitigation ⁽¹⁾	1.00	LS	\$200,000.00	\$200,000.00
18	HMA CL. 1/2" PG 64-22	68	Ton	\$100.00	\$6,800.00
19	Porous Asphalt Concrete	450	Ton	\$110.00	\$49,500.00
20	Detectable Warning Surface	72	SF	\$50.00	\$3,600.00
21	Remove/Relocate and Install New Signage	1	LS	\$4,500.00	\$4,500.00
22	Handrailing (along walls)	1,660	LF	\$25.00	\$41,500.00
23	Box Culverts	5	EA	\$2,000.00	\$10,000.00
24	Benches	2	EA	\$500.00	\$1,000.00
25	Gravity Block Wall	3,320	SF	\$25.00	\$83,000.00
26	Bridge	360	SF	\$200.00	\$72,000.00
27	Paint Line	250	LF	\$3.00	\$750.00
28	Removeable Bollard	8	EA	\$1,000.00	\$8,000.00
29	Fixed Bollard	4	EA	\$1,000.00	\$4,000.00
Subtotal Estimated Construction =					\$757,982.50
Contingency @ 15% =					\$113,697.38
Subtotal =					\$871,679.88
Construction TOTAL =					\$871,679.88
(1) Based on highly conceptual initial layout (\$70,000 for plantings and 130,000 for site preparation)					



- LEGEND**
- Existing Interurban Trail
 - Existing Sidewalks with Bike Lanes
 - Existing Neighborhood Trail
 - Proposed Segment Interurban Trail
 - Future Pacific/Edgewood Trail

Figure 1: City of Pacific Interurban Trail Extension

Section V.e. Total Estimated Project Cost and Schedule

Please fill in as many rows as needed in the tables below, to fully and accurately reflect your project's estimated costs and funding sources for each phase.

Project Sponsor	City of Pacific
Project Title	Interurban Trail Extension - 3rd Ave SW to County Line Road

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Planning			\$ -
Planning			
Planning			

Planning TOTAL: \$ -

Estimated Planning Completion Date (month and year): _____

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
PE/Design	PSRC	Secured	
PE/Design	Local	Secured	
PE/Design			

Preliminary Engineering / Design TOTAL: \$ -

Estimated PE/Design Completion Date (month and year): 6/1/2015

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Right of Way	PSRC	Reasonably Expected	\$ -
Right of Way	Local	Secured	\$ -
Right of Way			

Right of Way TOTAL: \$ -

Estimated ROW Completion Date (month and year): 6/1/2015

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Construction	PSRC	Reasonably Expected	\$ 865,000
Construction	Local	Reasonably Expected	\$ 135,000
Construction			

Construction TOTAL \$ 1,000,000

Estimated Construction Completion Date (month and year): 9/1/2016

Phase	Funding Source(s) (i.e. PSRC, state, local, etc.)	Secured / Reasonably Expected / or Unsecured*	Amount
Other			
Other			

Other TOTAL: \$ -

Estimated Other Completion Date (month and year): _____

TOTAL Estimated Project Cost, All Phases: \$ 1,000,000
Estimated Project Completion Date (month and year): 9/1/2016

* Additional information on these categories may be found at
<http://www.psrc.org/assets/11214/FinancialConstraintGuidance.pdf>.

After Recording Return To:

Puget Sound Energy, Inc.
Corporate Facilities Department, PSE-10S
P.O. Box 97034
Bellevue, WA 98009-9734
Attn: WBressler



20080612000618

PUGET SOUND EN EAS 94.00
PAGE001 OF 011
06/12/2008 11:03
KING COUNTY, WA

E2350059

06/12/2008 10:58
KING COUNTY, WA
TAX \$530.10
SALE \$29,500.00 PAGE001 OF 001

RECREATIONAL TRAIL EASEMENT AND AGREEMENT

REFERENCE #:

GRANTOR: PUGET SOUND ENERGY, INC.,

GRANTEE: CITY OF PACIFIC

SHORT LEGAL: S ½ Section 35, Township 21 North, Range 4 East, W.M.

ASSESSOR'S PROPERTY TAX PARCEL: 3353402000 and 3521049046

This RECREATIONAL TRAIL EASEMENT AND AGREEMENT ("Agreement"), dated as of this 4TH day of JUNE, 2008, is made and entered into by and between PUGET SOUND ENERGY, INC., a Washington corporation ("PSE"), and CITY OF PACIFIC, a Washington municipal corporation ("CITY").

RECITALS

A. PSE is the owner of rights with respect to a strip of land approximately one hundred (100) feet in width running through Section 35, Township 21 North, Range 4 East, W.M., in King County Washington more particularly described on Exhibit A attached hereto and incorporated herein by this reference the ("Property") herein.

B. PSE uses and intends to continue to use the Property for one or more utility systems for purposes of transmission, distribution and sale of gas, electricity, communications and other network commodities or services together with all necessary or convenient appurtenances thereto. In connection with such systems, PSE may construct, operate, maintain, repair, replace, improve, upgrade, remove, enlarge, and use any and all necessary or convenient appurtenances, incidents, or facilities, whether overhead, underground, semi-buried, or ground mounted, including, without limitation: poles and/or towers with crossarms, braces, guys and anchors; electric transmission and distribution lines; fiber optic cable; communication and signal lines and towers; pads, transformers, switches, and substations; underground conduits; cables; vaults; manholes and gas pipelines and appurtenances ("PSE's Facilities").

C. The CITY desires an easement to construct, operate, maintain, alter, repair, replace, improve, enhance, remove and enlarge and use the Property for certain recreational trails ("Recreational Trails") for the benefit of the general public.

NOW THEREFORE, for and in consideration of the mutual covenants contained herein and other good and valuable consideration the receipt and sufficiency of which are hereby acknowledged, PSE conveys and quit claims to the CITY a perpetual, nonexclusive easement ("Easement") upon, over, along, and across the Property (and any after-acquired title thereto) for such purposes, and subject to such terms and conditions, as are hereinafter set forth:

The terms "Easement" and "Easement Area" in this Agreement refer to the easement herein granted on the Property as described on attached Exhibit A.

Section 1.

Purpose. Subject to the terms and conditions of this Agreement, the CITY shall have the right to use the Easement Area for the purpose of developing, constructing, maintaining, repairing, altering and operating (and permitting the public to use for recreational purposes) the Recreational Trails (the "Permitted Use") and for no other purpose.

Section 2. Manner of Exercising the Permitted Use.

2.1 The CITY shall undertake the Permitted Use in a manner which does not disturb, interfere with, or otherwise impair the safe, reliable, continuous and normal commercial operation of PSE's Facilities by PSE. Without limiting the generality of the foregoing, the CITY shall at all times:

- (a) conduct the Permitted Use within the Easement Area with the appropriate degree of care so as not to materially damage, harm, disturb or impair the structural integrity of PSE's Facilities;
- (b) conduct the Permitted Use so as to maintain those clearances from PSE's Facilities set forth in plans for the CITY's improvements approved by PSE in the manner set forth in Section 2.6 hereof;
- (c) take all reasonable precautions to ensure that no persons engaging in the Permitted Use or any of their property come into contact with PSE's Facilities; and

(d) conduct the Permitted Use so as to provide a physical and legal access to PSE's Facilities, sufficient to allow the free passage of all equipment, vehicles and personnel as may be necessary for the construction, operation, maintenance, repair, replacement, removal, improvement, upgrade, enlargement, and use of PSE's Facilities, as provided in Section 3 hereof. In addition to and without limiting any other rights or remedies available to PSE, if the Permitted Use shall damage, harm, disturb, interfere with or otherwise impair the safe, reliable, continuous and normal commercial operation of PSE's Facilities, the CITY shall immediately: (i) cease and discontinue such Permitted Use; and (ii) notify PSE of the damage, harm, disturbance, interference or impairment. The CITY shall be allowed a reasonable opportunity to cure the damage, harm, disturbance, interference or impairment (or arrange, when PSE deems appropriate, for PSE to cure at the CITY's sole risk and expense), and shall be required to provide PSE with adequate assurance, as PSE determines in its sole discretion, that such damage, harm, disturbance, interference or impairment shall not reoccur. If such cure is made and such assurances are given, PSE shall notify the CITY when the Permitted Use may resume. PSE's acceptance of the cure and assurances will be presumed if the CITY is not notified of rejection within three days of notice to PSE of the cure or assurances whichever is later. During any period of damage, harm, disruption or interference with PSE's facilities which requires the closure of a portion of the Recreational Trails, the CITY may, if circumstances allow, reroute the Recreational Trails around the affected section. Any reroute of the Recreational Trails shall be temporary, and must cease immediately after the affected section is reopened. Any plan to reroute the affected section of the Recreational Trails must be approved by PSE. Such approval will not be unreasonably withheld or delayed.

2.2 The CITY shall, at all times, cause the Permitted Use to be undertaken with a degree of care appropriate for operating a public recreational trail facility within operating utility property used as a utility corridor so as to protect against bodily harm to persons and damage to property.

2.3 Vehicular use of the Easement Area is prohibited (except for bicycles, emergency vehicles and such vehicles owned or operated by, or on behalf of: (i) PSE; or (ii) the CITY in connection with activities authorized by this Agreement).

2.4 Upon completion of the Recreational Trails system, the CITY shall maintain and repair the Easement Area (and any improvements placed thereon by or on behalf of the CITY) as necessary to keep the same in a neat, clean and safe condition and otherwise suitable for the Permitted Use. During the initial construction of the Recreational Trails the CITY shall maintain the Easement Area in a reasonably safe condition so as to minimize situations that may cause injury to the public.

2.5 The CITY may temporarily close all, or portions of, the Easement Area to the general public for purposes of performing its responsibilities under this Agreement or otherwise to facilitate the Permitted Use. However, notwithstanding such closures, PSE (and its employees, agents, contractors and designated representatives) shall have the right to enter upon the Easement Area at all times whenever necessary, (provided that it shall conduct its activities in a manner that interferes with the CITY's use to the least extent practicable.) The CITY shall give PSE at least ten (10) days' advance written notice of any such closure (except closures required for emergencies). During such closures, PSE shall cooperate with the CITY so as to reasonably accommodate the CITY's activities within the Easement Area to the extent practicable.

2.6 The CITY shall not undertake or cause to be undertaken any development, construction, modification, alteration or other change to the Recreational Trail within the Easement Area without first obtaining PSE's prior written consent. Such consent may be conditioned upon PSE's review of and satisfaction with detailed plans and specifications, scheduling or such other matters relating to the Permitted Use. PSE shall not unreasonably withhold its approval. PSE shall give or deny its consent as soon as is practicable, but in no case more than 60 days from the date of receipt of said plans and specifications, unless within such 60 day period PSE notifies the CITY that additional time will be necessary and PSE continues its review process with reasonable diligence. PSE's failure to respond in such 60 day period shall be deemed to be its disapproval. The CITY acknowledges PSE's historical and continuing use of the Easement Area as operating utility property and covenants that any Recreational Trails design, plans and specifications submitted by the CITY to PSE will: (i) provide for the continuing use, operation, repair, replacement, modification and upgrade of, together with access to, any elements of, PSE's Facilities within, upon and under the Easement Area at the time of such submission to PSE; (ii) avoid any and all impacts on PSE's Facilities. In all events, the CITY shall be fully responsible to PSE for the acts or omissions of any person, servant, employee, contractor, subcontractor (of any tier), consultant, representative, or licensee, as fully and to the same extent as if such acts or omissions were acts or omissions of the CITY as more fully provided in Section 4 hereof.

2.7 The CITY shall not cause or permit any equipment, supplies, vehicles or other items to be kept or stored upon the Easement Area without first obtaining PSE's prior written consent thereto. Upon completion of activities which may disturb the surface or subsurface of the Easement Area, the CITY shall restore the Easement Area to a condition as good or better than the condition the Easement Area was in prior to such disturbance. If and to the extent the CITY may cause any excavations within the Easement Area, such excavations shall be done and completed with the highest degree of care appropriate to such public recreational trail construction.

2.8 The CITY shall at all times undertake the Permitted Use and all activities in accordance with this Agreement and in compliance with all applicable laws, regulations, rules, or ordinances of any governmental authority with jurisdiction, and the CITY shall obtain all necessary permits, approvals, or licenses required by any governmental agency with jurisdiction to undertake any activity on the Easement Area.

2.9 The CITY shall promptly pay, or cause prompt payment to be made to, and secure the discharge of any liens against the Easement Area asserted by, all persons and entities furnishing any labor, equipment, services, supplies, materials or other items in connection with the Permitted Use.

2.10 The CITY shall be solely responsible for ensuring compliance with all restrictions on the Permitted Use of the Easement Area, whether such restrictions are imposed or required by this Agreement or by applicable statute, rule, regulation, ordinance, or other law of any governmental agency with jurisdiction.

Section 3. PSE's Reserved Rights.

3.1 Right To Use the Easement Area

3.1.1 PSE has historically used, and continues to use, the Easement Area as operating utility property in connection with the transmission and distribution of electricity or gas, and any other purpose for which energy can be used as well as communication facilities and other network commodities. PSE reserves the right to use the Easement Area for such use and may modify, change or add to its facilities within the Easement Area as it wishes, and the CITY will relocate or modify its Recreational Trail to accommodate PSE's modifications, changes and (whether for electrical or gas utility purposes) additions upon 60 days written notice from PSE. The CITY's relocation or modification shall be at the CITY's expense only in the event such relocation or modification is to accommodate an activity associated with PSE's utility operations and as long as (a) PSE (following consultation with the CITY) has reasonably investigated options that do not require moving the Recreation Trail facilities and has concluded that such options are materially less desirable to PSE due to cost, constructability, functionality, operation, maintenance or other relevant factors, (b) PSE has exercised reasonable efforts to design its modifications to accommodate the CITY's trail use and minimize the CITY's relocation expenses; and (c) (unless emergency circumstances do not permit) sufficient time has been afforded the CITY to relocate the Recreational Trail.

3.1.2 Except and only to the extent specifically provided in Section 3.1.1 or elsewhere in the Agreement, to the maximum extent permitted by law, the CITY, on behalf of itself and its successors and assigns, covenants not to directly or indirectly, in any form or manner, oppose, protest, inhibit, obstruct, discourage, hinder, restrain, prevent or otherwise impair or interfere with: (i) the development, construction, operation, maintenance, repair, placement, replacement, improvement, movement, removal, enlargement, or use of any element of PSE's Facilities on or about the Easement Area or (ii) the exercise of any of the rights reserved to PSE under this Agreement. This paragraph 3.1.2 shall not be interpreted to limit, impair or affect the CITY's police power, nor to require the CITY to make any particular determination in any permitting process in which the CITY has review or decision-making authority over any proposal regarding the development, construction, operation, maintenance, repair, placement, replacement, improvement, movement, removal, enlargement, or use of any element of PSE's Facilities on or about the Easement Area.

3.2 Without limiting the rights reserved in paragraph 3.1 above, PSE reserves the right, to be exercised by giving notice to the CITY in accordance with paragraph 8.6, below, to temporarily close the Easement Area, or cause the CITY to temporarily close the Easement Area, in the event PSE determines that such closure is reasonably necessary for PSE to exercise its reserved rights in the Easement Area. Such closure shall be for the shortest period reasonably practicable in PSE's judgment. In such event, PSE may, at its option, request that the CITY close to the general public that portion of the Easement Area identified in PSE's notice, and the CITY, within ten (10) days' after receipt of PSE's notice for the CITY to do so, shall close to the general public that portion of the Easement Area identified in PSE's notice and keep such area closed during the period or periods of time requested by PSE; provided however, that if PSE's notice indicates that such portion of the Recreational Trails must be closed immediately, the CITY shall immediately undertake best efforts to so close such portion of the Easement Area. The CITY may, in the event a portion of the Easement Area is closed by PSE, reroute the portion of the Recreational Trails affected by the closure. Any reroute of the Recreational Trails must be approved by PSE, and shall not continue after the Easement Area is reopened. Approval shall not be unreasonably withheld or delayed.

3.3 PSE reserves the right to assign, convey, apportion, mortgage, hypothecate, transfer or otherwise grant any and all rights it may now have or may in the future acquire with respect to the Property including any and all of the rights reserved under this Agreement, to any third party, without notice or obligation to the CITY. If the Property or any interest therein is transferred by PSE to a third party, the third party shall be bound by all PSE's obligations under this Agreement.

3.4 In the event the CITY fails to perform any of its obligations arising under this Agreement, PSE may (without obligation to do so) perform the same at the CITY's sole risk and expense; provided, however, that (except in situations where PSE deems more expedient action to be appropriate, or where such failure interferes in PSE's judgment with the safe and reliable operation, maintenance or use of PSE's Facilities) PSE shall have given the CITY sixty (60) days' advance written notice, which notice shall specifically state the obligation the CITY has failed to perform and that within sixty (60) days after such notice, the CITY has not performed such obligation (or if such obligation cannot be reasonably accomplished within such sixty-day period, the CITY has not commenced performance of such obligation and diligently and assiduously prosecutes such obligation to completion) and has not taken or commenced action in good faith to prevent such failure to perform such obligation from recurring.

Section 4. Indemnity and Hold Harmless.

4.1 The CITY agrees to indemnify and hold harmless PSE, its directors, officers employees, agents, servants and representatives (and the respective successors and assigns of each and all of the foregoing) (collectively, the "Indemnitees") as provided herein to the maximum extent possible under law. Accordingly, the CITY agrees for itself, its successors and assigns to defend, indemnify, and hold harmless the Indemnitees from and against liability for all claims, liens, demands, suits, and judgments including costs of defense thereof for injury to persons, death, or property damage which is caused by, arises out of, or is incidental to the CITY's negligent exercise of rights and privileges granted by this Agreement. In the event it is necessary for PSE to incur attorney's fees, legal expenses, or other costs to enforce the provisions of this section, all such fees, expenses, and costs shall be recoverable from the CITY to the extent such items result from the negligence of the CITY. In the event it is determined that RCW 4.24.115 applies to this Agreement, the CITY agrees to defend, hold harmless, and indemnify PSE to the maximum extent permitted thereunder. In the event it is necessary for the Indemnitees to incur attorney's fees, legal expenses, or other costs to enforce provisions of this section, all such fees, expenses, and costs shall be recoverable from the CITY to the extent such items result from the negligence of the CITY.

4.2 As between the parties and for purposes only of the obligations herein assumed, the CITY waives any immunity, defense or other protection that may be afforded to it by any workers' compensation, industrial insurance or similar laws (including, but not limited to, the Washington Industrial Insurance Act, Title 51 of the Revised Code of Washington).

4.3 The parties intend for PSE and the CITY to be afforded the benefits of RCW 4.24.200 and RCW 4.24.210; provided however, in no event shall RCW 4.24.200 and RCW 4.24.210 act as, or be deemed to provide, a limitation of liability of the CITY to PSE under this Agreement.

4.4 The CITY's obligation to indemnify PSE pursuant to this Section 4 shall survive any termination of: (a) this Agreement pursuant to Section 7 hereof, or (b) any other termination of the CITY's rights to the Easement Area.

Section 5. Insurance.

5.1 Prior to undertaking any aspect of the Permitted Use, the CITY shall provide to PSE evidence (in a form acceptable to PSE) that the CITY (and, in the event the CITY hires a contractor to perform the Permitted Use, the CITY's contractor) has obtained commercial general liability policies of insurance (including contractual liability coverage) satisfactory to PSE and naming the Indemnitees as "Additional Insureds", and with limits no less than the following:

Bodily Injury Liability, including Automobile Bodily Injury Liability	\$10,000,000 each occurrence
Property Damage Liability, including Automobile Property Damage Liability	\$10,000,000 each occurrence
Employer's Liability (Stop Gap)	\$10,000,000 each occurrence

Such insurance shall be maintained at all times by the CITY (or the CITY's contractors, if any). The CITY shall ensure that any policies of insurance that it carries pursuant to this Agreement as insurance against property damage (including personal property, fixtures or improvements) and against liability for personal injury (including death) shall include a provision therein providing a waiver of the insurer's right to subrogation against the Indemnitees. To the extent permitted by such policies, the CITY hereby waives such rights of subrogation.

5.2 With respect to any insurance requirements imposed directly upon the CITY under this Section 5, the CITY may substitute evidence that the CITY is self insuring such risk. As to any other insurance requirement (e.g., a requirement imposed upon a Contractor), PSE may, in its sole discretion, accept evidence of prior-approved self insurance to evidence compliance with the insurance requirements of this Section 5.

5.3 Procurement and maintenance of such insurance, or PSE's acceptance of prior-approved self insurance, shall in no way effect or limit the CITY's obligations under this Agreement, including, without limitation, its indemnification obligations.

5.4 The CITY, a charter city government under the constitution of the State of Washington, maintains a fully funded Self Insurance program as defined in CITY Code _____ for the protection and handling of the CITY's liabilities including injuries to persons and damage to property.

5.5 PSE acknowledges, agrees and understands that the CITY is self-funded for all of its liability exposures. The CITY agrees, at its own expense, to maintain, through its self-funded program, coverage for all of its liability exposures for this Agreement. The CITY agrees to provide PSE with at least 30 days prior written notice of any material change in the CITY's self-funded program and will provide PSE with a certificate of self-insurance as adequate proof of coverage. PSE further acknowledges, agrees and understands that the CITY does not purchase Commercial General Liability insurance and is a self-insured governmental entity; therefore the CITY does not have the ability to add the Company as an additional insured.

5.6 Any City insurance or self-insurance shall be (and provide that it is) primary and any insurance coverage (if any) maintained by PSE shall be (and City insurance or self-insurance shall acknowledge that any such PSE insurance coverage is) excess and non-contributing to the City's insurance coverage.

Section 6. No Warranties/Acceptance "AS IS".

6.1 PSE does not warrant title to the Easement Area and shall not be liable for defects thereto or failure thereof. PSE does not warrant, and expressly disclaims all warranties (express or implied) regarding, the condition of the Easement Area, including without limitation, its environmental condition. The CITY has inspected the Easement Area, and has had an opportunity to have the Easement Area inspected by experts of the CITY's own choosing qualified to discover patent, latent, known, and unknown defects in PSE's title to the Easement Area, and in the condition, including environmental condition, of the Easement Area, and the CITY hereby accepts the same in its "as is," "where is," condition, including, without limitation, its environmental condition, and with all faults, defects or deficiencies whether patent, latent, known, or unknown, without recourse to PSE of any kind; provided, however, that PSE shall not be released from, and CITY does not accept, any liability to third parties which may result from (1) the release of hazardous materials that occur prior to the date of the conveyance of the Easement hereby except to the extent now or hereafter caused or exacerbated by the CITY or the Permitted Use, or (2) the release of hazardous materials by PSE at any time, except to the extent now or hereafter caused or exacerbated by the CITY or the Permitted Use.

6.2 Any plans, specifications, schedules, documents or other information (collectively, "Submittal") provided by the CITY to PSE pursuant to this Agreement are for PSE's informational purposes only. PSE's receipt of any such Submittal, or any review, analysis, consent, approval, or other consideration or disposition thereof by PSE, or PSE's failure to review, analyze, consent, approve or otherwise consider or dispose of any such Submittal (including, without limitation, failure to discover any error, defect or

inadequacy of such Submittal) shall not relieve the CITY of any of its obligations under this Agreement. PSE hereby expressly disclaims any and all warranties, express or implied, with respect to any one or more Submittals, or any other plans, specifications, documents or information developed, reviewed or provided by PSE to the CITY as a condition of this Agreement.

Section 7. Termination.

7.1 In the event of any material breach by the CITY of any of the terms or provisions herein, in addition to all other rights or remedies it may have, PSE may terminate this Agreement by sixty (60) days' advance written notice, which notice shall specifically state the reason for such termination. No such termination, however, shall be effective if, within sixty (60) days after such notice, the CITY has cured the breach (or if cure cannot be reasonably accomplished within such sixty (60) day period, the CITY has commenced such cure and diligently and assiduously prosecutes such cure to completion) and has taken or commenced action in good faith to prevent such breach from recurring.

7.2 Following initial construction of the Recreational Trails, this Agreement and all of the CITY's rights hereunder shall terminate and revert to PSE upon abandonment of the Permitted Use by the CITY. The CITY shall be presumed to have abandoned the Permitted Use if the CITY and/or their assigns ceases to maintain the Easement Area for a period of five (5) successive years. No abandonment shall occur due to any delay in initially constructing the Recreational Trails.

7.3 No termination of this Agreement shall release the CITY from any liability or obligation: (a) with respect to the CITY's indemnification obligations pursuant to Section 4 of this Agreement; or (b) with respect to any matter occurring prior to such termination.

Section 8. Miscellaneous.

8.1 Assignment

The CITY may not assign this Agreement or any portion of its rights under this Agreement without PSE's prior written consent, which shall not be unreasonably withheld if such assignee is a public agency or governmental body with the authority and capacity to perform the CITY's obligations hereunder. Any assignment of the CITY's rights or interests in and under this Agreement without such consent shall be null, void and without effect. It shall not be deemed unreasonable for PSE to withhold its consent to an assignment if the assignment lacks an assumption by the assignee of all of the CITY's obligations under this Agreement.

8.2 Successors

The CITY's rights and obligations under this Agreement shall not be assigned except as provided in paragraph 8.1, above. Subject to the foregoing, this Agreement and the terms and conditions contained herein shall be binding upon and shall inure to the benefit of the parties hereto and their respective successors and assigns.

8.3 Local, State and Federal Laws

The CITY shall at all times comply with any and all applicable laws, rules, regulations and ordinances.

8.4 Consent to Jurisdiction and Venue

No party shall commence or prosecute any claim, proceeding, suit or legal action to enforce this Agreement, to recover damages for breach of or default under this Agreement or otherwise arising under or by virtue of this Agreement, other than in the courts of the State of Washington, or the United States District Court, Western District, State of Washington. Each party hereby irrevocably consents to the jurisdiction of the courts of the State of Washington with venue laid in King County or the United States District Court, Western District at Seattle, State of Washington.

8.5 Notices

Any notice, request, approval, designation, direction, statement or other communication (collectively, "Notice") required or permitted to be given under this Agreement shall be in writing and shall be delivered in person or mailed, properly addressed and stamped with the required postage to:

PSE: Puget Sound Energy, Inc
Corporate Facilities Department
PO Box 97034
Bellevue, WA 98009-9734
Attn: Director Corporate Facilities

CITY: City of Pacific
100 3rd Avenue Southeast
Pacific, WA 98047

Either party may, from time to time, change such address by giving the other party Notice of such change in accordance with this paragraph 8.5. All parties shall keep the others apprised, by written notice given in accordance with this paragraph 8.5.

8.6 No Partnership

This Agreement shall not be interpreted or construed to create an association, joint venture or partnership between the parties or to impose any partnership obligations or liability upon either party. Neither party shall have any right, power or authority to enter into any agreement or undertaking for or on behalf of, to act as or be an agent or representative of, or to otherwise bind the other party.

8.7 Nonwaiver

The failure of either party to insist upon or enforce strict performance by the other party of any of the provisions of this Agreement or to exercise any rights or remedies under this Agreement shall not be construed as a waiver or relinquishment to any extent of such party's right to assert or rely upon any such provisions or rights in that or any other instance; rather, the same shall be and remain in full force and effect.

8.8 Nonmerger; Survival

All obligations of the parties and all other provisions of this Agreement which may reasonably be interpreted or construed as surviving the conveyance of the easement contemplated by this Agreement, or the expiration or termination of this Agreement, including without limitation, the CITY's obligations to restore the Easement Area and obligations to indemnify PSE, shall survive the conveyance of the easement contemplated by this Agreement and the expiration or termination of this Agreement.

8.9 Entire Agreement

This Agreement sets forth the entire agreement of the parties. This Agreement shall be construed as a whole. All provisions of this Agreement are intended to be correlative and complementary. The misplacement, addition or omission of a word or character shall not change the intent of any part of this Agreement from that set forth by this Agreement as a whole.

8.10 Headings

The headings of sections, subsections and paragraphs of this Agreement are for convenience of reference only and are not intended to restrict, affect or be of any weight in the interpretation or construction of the provisions of such sections, subsections or paragraphs.

8.11 Nonseverability

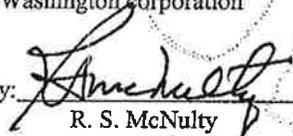
In the event any provision of this Agreement is held to be invalid or unenforceable by any court of competent jurisdiction, this Agreement shall be null and void, and the parties shall be returned to their respective positions immediately prior to executing this Agreement.

Dated as of the date first written above.

PSE:

Puget Sound Energy, Inc.,
a Washington corporation

By:



R. S. McNulty
Director Corporate Facilities

THE CITY:

City of Pacific,
a Washington municipal corporation

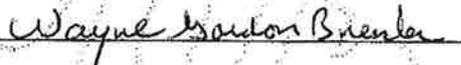
By:



STATE OF WASHINGTON)
) SS
COUNTY OF KING)

On this 4TH day of JUNE, 2008, before me, the undersigned, a Notary Public in and for the State of Washington, duly commissioned and sworn, personally appeared R.S. McNulty, to me known to be the person who signed as Director Corporate Facilities of PUGET SOUND ENERGY, INC., the corporation that executed the within and foregoing instrument, and acknowledged said instrument to be the free and voluntary act and deed of said corporation for the uses and purposes therein mentioned; and on oath stated that he was duly authorized to execute said instrument and that the seal affixed, if any, is the corporate seal of said corporation.

IN WITNESS WHEREOF I have hereunto set my hand and official seal the day and year first above written.



Notary Public in and for the State of Washington

WAYNE GORDON BRESSLER

[Type/print Name]

Residing at BELLEVUE

My commission expires: 8/15/2010

Recreational Trail Easement and Agreement
Puget Sound Energy – Grantor
City of Pacific – Grantee

EXHIBIT A
to
RECREATIONAL TRAIL EASEMENT AND AGREEMENT

PARCEL 1:

That portion of the hereinafter described Parcel "A" lying within a strip of land 100 ft in width, the Easterly line of which strip is a line drawn parallel with and 325 Easterly, when measured at right angles and/or radially, from the survey line of State Highway Route 167;

Parcel "A"

Lots 12 through 17 and Lots 26 through 31, Block 9, and Lots 1 through 12, inclusive, and Lots 40, 41, 42, and 43, Block 10, C.D. Hillman's Pacific City addition to the City of Seattle; Division No. 1, according to the plat thereof recorded in Vol. 13 of Plats, page 64, records of said County; TOGETHER WITH any right, title or interest of the State of Washington in and to any streets or alleys adjoining said lots.

PROPERTY 2:

That portion of the hereinafter described Parcel "A" lying within a strip of land 100 ft in width, the Easterly line of which strip is a line drawn parallel with and 325 ft Easterly, when measured at right angles and/or radially, from the survey line of State Highway Route 167.

Parcel "A"

That portion of the South 1/2 of the Southwest 1/4 of the Southeast 1/4 of Section 35, Township 21 North, Range 4 E WM, lying Northeasterly of that 100 ft wide strip of land condemned for the Pacific Coast Power Company's R/W in King County Superior Court Case No. 82340;

ALSO

That portion of the North 1/2 of the Southwest 1/4 of the Southeast 1/4 of said Section lying South of the North 30 ft thereof conveyed to King County for road by instrument recorded under Auditor's File No. 3260960 records of said County.

